



I. Executive Summary

Graduate students in the University of Washington Urban Design and Planning program (UW) worked with the Delridge Neighborhoods Development Association (DNDA) and the West Seattle community group Dells and Ridges to map existing neighborhood trails and recommend routes for pedestrian “connectors.” Final suggestions for trail improvement include signage designs, wayfinding strategies, new trail construction, and upgrades to existing informal trails. The final goal of this working partnership is that our recommendations will coincide with the mission of Dells and Ridges to “...improve the quality of life of Delridge residents by linking neighborhoods, Longfellow Creek Legacy Trail, Riverview Trail, schools, cultural resources, businesses, and parks within the District...”

Proposed Connectors:

- Duwamish – Legacy Connector: Links the Legacy Trail with the paved Duwamish Trail, as well as with bike paths to downtown Seattle
- Alki – Legacy Connector: Links the Legacy Trail to the Alki Trail, a shared-use path that extends around the West Seattle peninsula
- Genesee Connector: Connects commercial development on 35th Ave., Delridge Way¹, the Pigeon Point neighborhood, and the Youngstown Cultural Arts Center
- Brandon Connector: Connects the Fairmount Playfield, Camp Long, the proposed Brandon Loop, South Seattle Community College (SSCC), Chinese Garden and Kellogg Island on the Duwamish River
- Graham Connector: Connects High Point, the Louisa Boren school, and SSCC
- Myrtle Connector: Connects the Delridge Valley to the Riverview Playfields and SSCC

¹ All streets in the Delridge Neighborhood have a suffix or prefix of SW, depending on if it is a north-south or east-west, respectively. For ease of reading, this suffix has been omitted from the following section.

1. Introduction

The Delridge neighborhood, as defined by the City of Seattle, is situated in the middle of two steep ridges, which makes east-west travel particularly challenging for pedestrians. There are several north-south pedestrian and bicycle corridors running through the neighborhood: the Longfellow Creek Legacy Trail, the Riverview Trail, and the Duwamish Trail. *Refer to Figure 2.1: Map of Existing Trails*

As mentioned, the steep topography of Delridge has restricted east-west corridors from connecting residential areas, businesses, and schools. UW Department of Urban Design and

Dells and Ridges Mission Statement: To guide the development of the Dells and Ridges Trail system to improve the quality of life of Delridge residents by linking neighborhoods, Longfellow Creek Legacy Trail, Riverview Trail, schools, cultural resources, businesses, and parks within the District (by producing a concept plan)

Planning was invited by DNDA to generate ideas for launching a central commercial space at the intersection of Delridge Way and Brandon Street, and to connect this space with the greater West Seattle area via pedestrian and bicycle corridors. DNDA requested tasks related to this problem, including mapping the Delridge neighborhood trail and staircase network, providing Geographic Information Systems (GIS) shapefiles of trails and staircases, recommending additional east-west

connectors, and presenting implementation strategies. Project goals focused on making the intersection of Brandon Street and Delridge Way accessible through a connected trail and staircase system. This work was done in conjunction with a West Seattle community group, Dells and Ridges, which was formed to identify non-motorized trail improvements and to establish east-west connectivity².

Research and Recommendations

Final recommendations regarding trail connectors are based on a combination of field observations and community input. Dells and Ridges has added input about their efforts to connect the three primary trail systems to each other and to link these trails with other trails in West Seattle. Research and recommendations are also geared toward the goals expressed in the 1999 Delridge Neighborhood Plan and the Visualize Delridge Community Meeting (March 2006), organized and hosted by UW and DNDA.

² Dells and Ridges membership includes DNDA, the National Park Service, Feet First, Seattle Parks and Recreation, Seattle Public Utilities, Riverview Trail Improvement Project, Seattle Housing Authority, Southwest District Council, Morgan Community Association, and the Longfellow Watershed Council

(This page is a placeholder for Figure 2.1: Map of Existing Trails)

(This page is a placeholder for Figure 2.1: Map of Existing Trails)

Trails

1.1 The 1999 Delridge Neighborhood Plan

Under the “Key Strategies” section (Ch. 2), the plan articulates the community’s goal of providing a “high quality system of multipurpose trails connecting and accessing significant environmental features, public facilities, and developed residential areas³.”

Objectives to measure this goal included:

- Creating a comprehensive system of walking trails
- Linking residential areas to parks and community facilities, schools, business nodes, and transit systems
- Extending trails through nature corridors providing a high-quality, diverse sampling of Delridge’s environmental resources
- Ensuring that trails are pedestrian friendly and safe

The plan suggested that these community objectives be achieved in cooperation with the City of Seattle. The system of east-west trail segments would be developed, and the City would take responsibility for maintaining the trails and mapping hill climbs in the area to define ownership and responsibility. Additionally, mapping the ownership of utility poles adjacent to the hill climbs would be useful in a comprehensive Seattle City Light strategy to create an energy-efficient lighting program for the safe movement of pedestrians.

All segments of a north-south trail system suggested in the 1999 Neighborhood Plan have been developed except for Puget Park and West Duwamish Greenbelt trails. Improvements on Delridge Way and the Legacy Trail have been accomplished according to an improvement classification system, while the Riverview Trail is currently under construction. Trail 8 starts at Highland Park Drive and spans the eastern edge of SSCC, continuing to Puget Park. However, since SSCC engages in truck-driving training along this route, the Riverview Trail Improvement Project (rTrip) is currently discussing relocating the trail on SSCC’s west lawn. Trail 9 has been established from Highland Park Way along the greenbelt hillside to Puget Creek which was discovered in a Riverview Trail feasibility study to be an undesirable location due to impacts on environmentally sensitive areas. Suggestions for east-west segments of a trail system included a Juneau St. trail and a Graham St. trail from High Point to SSCC. Graham St. is currently usable as a pedestrian connector from High Point to SSCC, but it lacks sufficient wayfinding, maintenance, and lighting to substantiate its presence. It would be problematic to develop Juneau St. as an east-west corridor due to private development in the public right-of-way. Refer to Figure 2.2 Walking Trails Update from Neighborhood Plan and Figure 2.3 Trail Classes from Neighborhood Plan

3 Delridge Neighborhood Plan. (1999). Seattle Government Department of Neighborhoods [WWW page]. URL <http://www.ci.seattle.wa.us/neighborhoods/npi/plans/delridge/>

Walking trails Update of 1999 Plan

North/South

- 1 Delridge Way - Class 1
- 2 Longfellow Creek Trail - Class 1
- 3 Pigeon Point/Puget Park Trail - Class 2
- 4 Cliff Court Loop Trail - Class 1
- 5 Highpoint Hillside Trail - Class 2
- 6 Highpoint Trail - Class 1
- 7 Longfellow Creek Trail South - Class 1
- 8 Puget Park/Riverview Trail - Class 1/2
- 9 Delwood Hillside Trail - Class 2

East/West

- 10 Charleston Street Trail - Class 1/3
- 11 Innes Street Trail - Class 1/3
- 12 Puget Boulevard Trail - Class 1/2
- 13 Juniper Street Trail - Class 1/3
- 14 Oak Street Trail - Class 1/3
- 15 Myrtle Street Trail - Class 1/3
- 16 Holden Street Trail - Class 1/3
- 17 Thistle Farm Trail - Class 1

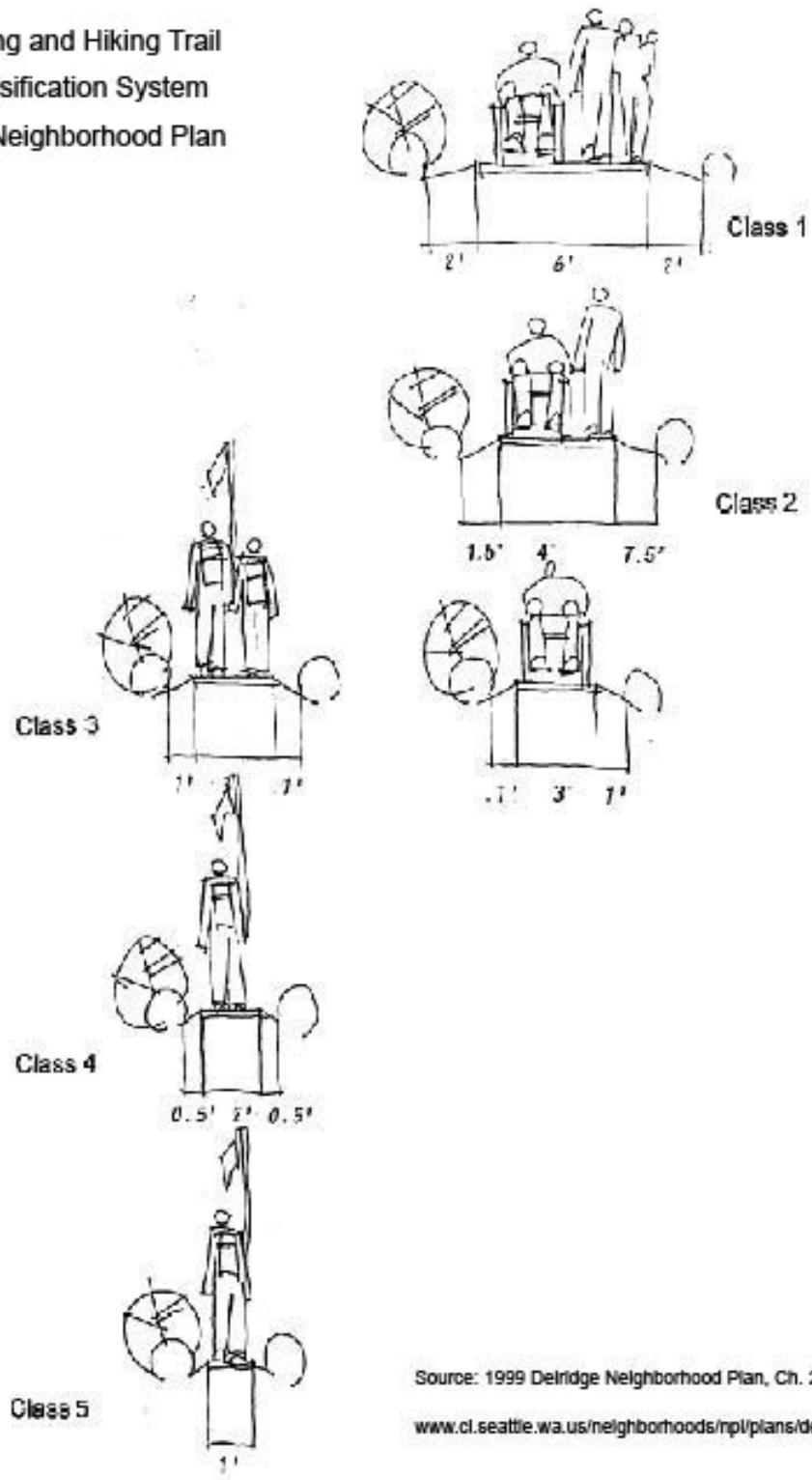
- Accomplished
- Recommended



Source: 1999 Delridge Neighborhood Plan, Ch. 2, p. 23 www.ci.seattle.wa.us/neighborhoods/npl/plans/delridge/

Figure 2.2: Walking Trails Update from Neighborhood Plan

Walking and Hiking Trail
Classification System
1999 Neighborhood Plan



Source: 1999 Delridge Neighborhood Plan, Ch. 2, p. 24
www.ci.seattle.wa.us/neighborhoods/npl/plans/delridge/

Figure 2.3: Trail Classes from Neighborhood Plan

Nodes

The neighborhood plan also designates a series of compact, mixed-use neighborhood “nodes” accessible from walkways, park trails, bikeways, transit routes, and local residential streets. The plan explicitly advocates completing the trail network to link business nodes, open space, and natural areas. The Neighborhood Plan’s Central Node, known in this plan as the Brandon Node, is located on Delridge Way between Puget Blvd. and Juneau St., and serves as a commercial district for the Puget Ridge and Cottage Grove neighborhoods, the Louisa Boren School, and other neighborhood businesses.

What is a node?

According to urban planner Kevin Lynch, a node is a focal point, an intersection or locus.

Source: Lynch, Kevin. (1960). *The Image of the City*. Cambridge Mass.: The Technology Press and Harvard University Press

Suggested design guidelines and amenities for the Brandon Node include expanding sidewalks, improving crosswalks, and installing landscaping, storefront awnings, benches, trash receptacles, bike racks, street trees, signage, and artwork. Neighborhood plan goals also focused on the use of these pedestrian amenities to create “gateways”

to the Brandon Node, particularly at the intersections of Delridge Way with the following roads: Puget Boulevard, and Brandon St., Findlay St., and Juneau St. Additional suggestions include a hill climb and park improvements to encourage pedestrian activity and aesthetic appeal on Brandon St. Additional pedestrian improvements include crosswalk curbing extensions, expanded crosswalk marking and paving, and median planters to calm traffic on Delridge Way. Some proposed projects, such as the DNDA mixed-use library and Delridge Way intersection changes have already been accomplished, and the proposed Brandon Mini Park is currently under construction. Refer to Figure 2.4: *Central Node Improvements from Neighborhood Plan*

1.2 Visualize Delridge Community Meeting

On March 2, 2006 DNDA and UW conducted a community meeting at Louisa Boren School (temporarily the home of Cleveland High School). The meeting affirmed the priorities, visions, and goals expressed in the neighborhood plan. Meeting participants shared several positive comments relating to the neighborhood walking environment. Among these was the large

Meeting Participant Suggestions: Make east-west connections to access neighboring communities; Connect the Alki Trail to Delridge Trails; Establish a peninsula-wide trail system; Improve lighting on stairways; Pave sidewalks; Mitigate traffic on Delridge Way; Create a trail connecting Puget Ridge to the Delridge Community Center

amount of green space, especially along Longfellow Creek. In contrast, participants voiced concerns about the lack of foot traffic along Delridge Way,

the minimal amount of area retail establishments, the lack of sidewalk continuity, the uninviting pedestrian atmosphere, “fast and crazy drivers,” gunshots, drug activity, prostitution,

Update on Proposed Central Node Improvements of 1999 Plan

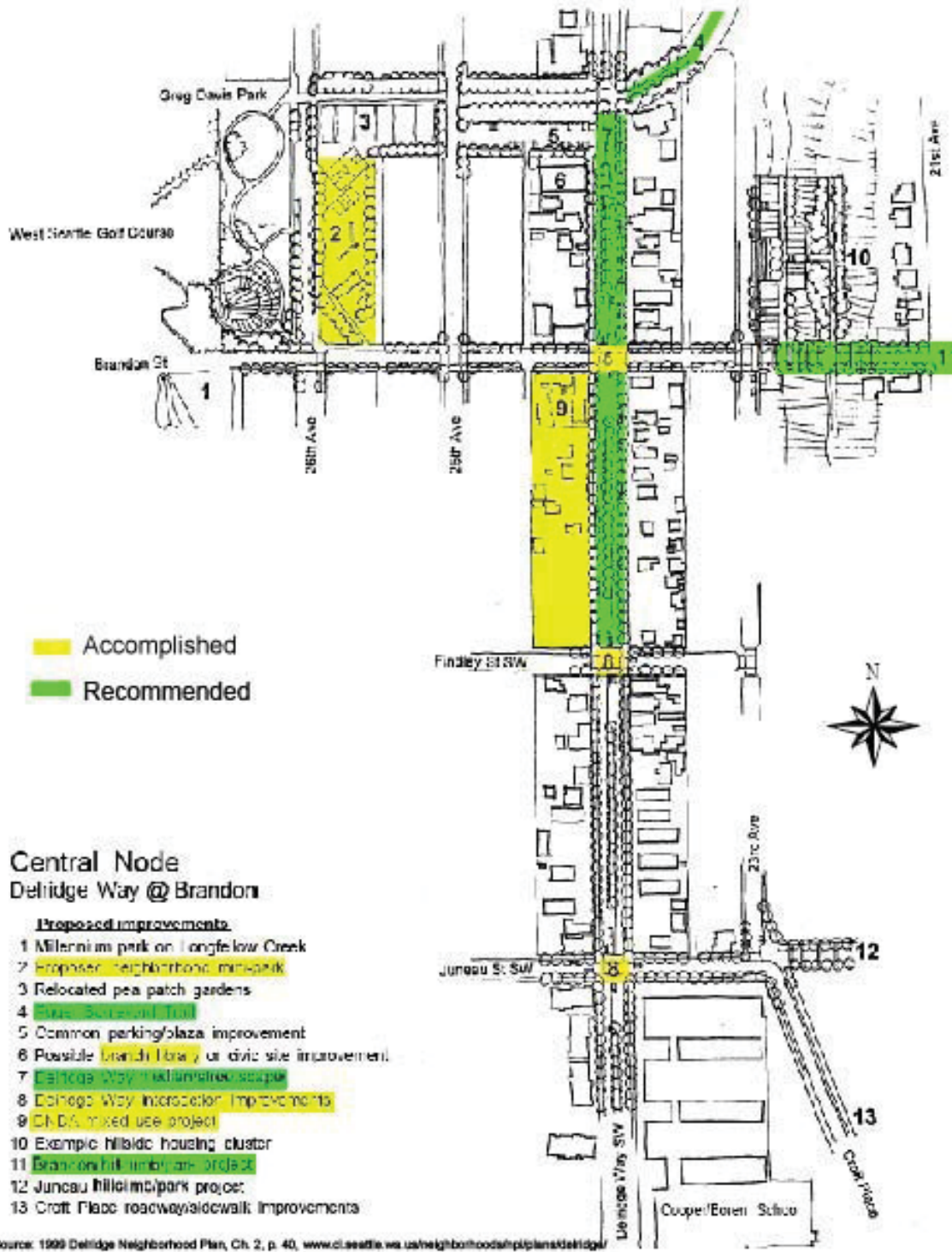


Figure 2.4: Central Node Improvements from Neighborhood Plan

nighttime safety, the seclusion of some trails and stairways, and the increasing levels of trash on the streets and stairs due to the “very few, undersized garbage cans throughout the community.”

1.3 Existing Conditions and Trail and Staircase History

Legacy Trail

Longfellow Creek is a four mile long, north-south stream, located in West Seattle’s Delridge and Westwood neighborhoods. The Legacy Trail brings pedestrians to Longfellow Creek and winds through public green spaces and street, linking community resources such as the Delridge Branch of the Seattle Public Library and the Westwood Village shopping center. Refer to Figure 2.1: Map of Existing Trails and Figure 2.5: Photo from the Legacy Trail. Trail improvements occur on a voluntary basis, an important focus of which is replacing non-native invasive plants with native plants. The National Park Service’s Rivers and Trails Program awarded DNDA a technical grant to develop a concept plan for the Longfellow Creek Legacy Trail and Riverview Trail connections (see Appendix 2 for a detailed background on the Legacy Trail).



Figure 2.5: Photo from the Legacy Trail

Riverview Trail

Formed in 2000, rTrip is committed to linking neighborhoods of Southwest Seattle via the Riverview Trail through a dedicated greenway to the Duwamish Trail and the regional trail network. The organization’s master plan designed the trail from Riverview Park to SSCC. The Riverview Trail was made a priority in the Delridge Neighborhood Plan, and in 2005 the trail was selected as a Green Seattle Partnership site. This designation makes it part of a collaborative effort between the City of Seattle and the Cascade Land Conservancy to raise public environmental awareness, attract increased financial and volunteer resources, remove ivy, replant native plants, and restore public forests (see detailed history in Appendix 2).



Figure 2.6: Photo from the Riverview Trail

The Duwamish Trail

The Duwamish Trail is a paved path along the eastern border of the West Seattle peninsula. Refer to figure 2.7: Photo from the Duwamish Trail. It is eleven miles long and follows the Duwamish River and West Marginal Way from Holden St. to Idaho Street where it ends about half-mile south of the West Seattle Bridge. A joint project between the City of Seattle and the Port of Seattle will complete a “missing link” between the Duwamish Trail and the Alki Trail, a vital connection for bicycle travel on the West Seattle peninsula. Refer to Figure 2.8: Duwamish Trail Connector. The Alki Trail also connects to Seacrest Dock, which runs a seasonal water taxi to Elliot Bay, providing a multi-modal transportation system to and from downtown Seattle. The City of Seattle will construct an asphalt path on the east side of West Marginal Way between Chelan Ave. and Idaho St., scheduled to begin construction in the summer of 2006 at an estimated cost of \$750,000.



Figure 2.7:: Photo from the Duwamish Trail

Existing Wayfinding

Wayfinding is the ability of a person to find his or her way to a given destination⁴. The Legacy Trail features brown signposts, gateways, placards, and tall trail markers for wayfinding. Similarly, metal signposts are the primary wayfinding method for the Riverview Trail. In general, the Duwamish Trail fails to provide adequate direction to the user. Although plans are underway to complete the Duwamish Trail and connect it to the Alki Trail, the Duwamish Trail currently dead-ends at train tracks at both Idaho St. and Andover St. The east-west corridors in Delridge require the most improvement, in terms of signage and wayfinding. There are numerous staircases that allow pedestrians to climb the steep ridges of Delridge, yet nearly all of these east-west corridors lack any guiding signage. Providing uniform and cohesive wayfinding designs and signage will help residents navigate the community.



Figure 2.8: Duwamish Trail Connector

4 Turning Point Signs & Design. (n.d.). Glossary of sign terms [WWW page]. URL <http://www.tpsigns.com/pages/glossary.html>



Figure 2.9: Photo from the Charlestown Staircase

Safety

As noted in the proceedings of the Visualize Delridge Community Meeting, residents feel there is a need to address safety concerns related to traffic, crime, limited visibility, seclusion on trails and stairways, and increasing levels of trash on the stairways. On the ground observations and resident input indicate that staircases and trails need increased maintenance to prevent brush overgrowth, as on the Charlestown Steps. Refer to Figure 2.9: Photo of Charlestown Staircase. Additional safety improvements include increasing trail width, installing more lighting, and providing railings on all staircases.

Disabled Accessibility

Although not mentioned in the Neighborhood Plan as a priority, observations show that older and disabled persons may have difficulty using trails and may need alternatives to stairways. A series of switchback ramps could address these needs, but sufficient space, slopes, and funds would be necessary to do so. Depending on the availability of these requirements, the Seattle Department of Transportation (SDOT) suggests that new accessible hill climbs be installed only where they are deemed necessary and where no alternative routes exist within a distance of one block. Pre-existing informal social paths that show high volumes of pedestrian traffic may, however, be candidates for the installation of typical stairways.

Staircase History

There are currently more than 25 individual staircases in the Delridge neighborhood (Refer to Figure 2.10: Map of Existing Staircases), indicating that local topography poses a significant challenge to traveling outside of the Delridge Valley. Many staircases were built as parcels were developed in West Seattle. According to data obtained from SDOT, it seems there was a boom in development in 1979, when staircases on the Graham, Holden, and Juneau St. rights-of-way were constructed. Another period of development may have been in 1947, when staircases on the Charlestown St. right-of-way were installed. The newest staircases to be installed in the neighborhood are located on the Myrtle St. right-of-way between 27th and 28th Ave.

(This page is a placeholder for Figure 2.10: Map of Existing Staircases)

(This page is a placeholder for Figure 2.10: Map of Existing Staircases)

2. Methodology

Trails were mapped with consumer-grade Garmin global positioning system (GPS) units. When the terrain afforded the GPS units unobstructed access to satellite signals, their stated accuracy was approximately fifteen feet. However, after comparing collected data with aerial photos, their actual accuracy in these conditions often appeared much better, within five feet of the actual trail. In contrast, when trees (and in a few cases buildings) interfered with satellite reception, the GPS units' stated accuracy was downgraded to approximately 50 feet. Because it was difficult to compare this data with aerial photos due to the tree cover, it cannot be concluded if their actual accuracy was any better than their stated accuracy. Thus, in areas with substantial tree cover, particularly the West Duwamish Greenbelt, the data should be regarded only as a rough approximation of ground-level features.

To convert the GPS data into a format usable in geographic information systems (GIS), a program that converts such data into shapefiles was downloaded from the Minnesota Department of Natural Resources⁵. The GIS program utilized for the project was ArcGIS 9.1. Once the data were in GIS, minor corrections were made where it was obvious that the resulting features deviated from specific locations in aerial photos. Because of the inaccuracy of the GPS units in the West Duwamish Greenbelt, our data were modified based on comparison with a Seattle Parks and Recreation GIS map of the area titled Riverview Playfield Area⁶. Also, data for the Camp Long trail system were based on another Seattle Parks and Recreation GIS map, this one titled Camp Long⁷. Other data for the project were from the Washington State Geospatial Database Archive (WAGDA)⁸ and the Delridge Neighborhood Plan.

While mapping trails, field notes were collected in order to document the existing condition of trails and stairs, trail construction and materials, wayfinding techniques, safety, lighting, maintenance, and any other notable considerations or opportunities to connect different trail systems. This information was compiled into the attribute tables of the GIS shapefiles for each trail. In addition to trail mapping, UW students met periodically with DNDA representatives and attended Dells and Ridges meetings in order to keep all stakeholders up-to-date on UW progress and vice versa.

5 Minnesota Department of Natural Resources. (2005). DNR Garmin Extension for ArcView [WWW page]. URL <http://www.dnr.state.mn.us/mis/gis/tools/arcview/extensions/DNRCGarmin/DNRCGarmin.html>

6 Riverview Trail Improvement Project (rTrip). GPS Trail Map [WWW page]. URL <http://www.rtrip.org>

7 Seattle Parks and Recreation. (2002). Camp Long Park Vegetation Management Plan [WWW page]. <http://www.ci.seattle.wa.us/parks/parkspaces/camplong/documents/CampLongVMP.pdf>

8 University of Washington Libraries. (n.d.). Washington State Geospatial Database Archive (WAGDA) [WWW page]. URL <http://wagda.lib.washington.edu>

3. Recommendations

3.1 Unified Network Wayfinding: An invitation for trail use

A strong trail network is one that people use regularly. One key to making sure people regularly use a trail network is inviting people to walk on it. This relatively simple step is often overlooked.

What is wayfinding?

"In its most literal sense, wayfinding is the ability of a person to find his or her way to a given destination. While the words and graphics on ... signs are important to the process, wayfinding also depends on the information inherent in [trail] design. Truly successful wayfinding ... integrate[s] both elements." *Source: Gable Signs & Graphics Inc, Glossary of "Sign Language: The Terminology of Signs" [WWW page]. URL <http://www.gablesigns.com/glossary/w.html>.*

People feel invited to walk on a trail network if:

- They know they have permission to be there: trails feel distinctly public rather than having the appearance of being private.
- They know something about the destination of the trail (people using the trail for commuting or other

transportation might need to know which destination the trail leads to, while recreational users might need to know only that the trail leads somewhere, rather than being a dead end).

- The trail is pleasant or interesting to walk on and it feels safe.

Permission

People know they have permission to walk on a trail if there are signs indicating they can and if the trails are maintained and well drained. These are important clues in distinguishing public from private land. Furthermore, if signs and trail conditions look similar from trail to trail, people will have a heightened sense that all of the trails are part of a unified network.

Destination

Information about destination can also be conveyed through words and graphics on signs and with easy-to-read maps. However, a direct line of sight is one of the most effective ways to convey destination and should be implemented wherever possible. To illustrate, examples of successful line-of-sight wayfinding include seeing the bottom of a staircase from the top or seeing where an east-west connector trail picks up again on the other side of Delridge Way. Destination information is particularly important in the Delridge neighborhood because many of the trails in the network exist as a mix of earthen trails, staircases, and streets.

Pleasant and Safe

Safe and pleasant trails and staircases are largely a function of the condition of the trails and staircases themselves. Conditions supporting these characteristics include sufficient width,

the absence of overgrowth, and open views of nearby areas to ease feelings of seclusion. Safe and pleasant trails and stairs also have amenities such as lights, benches, garbage cans, points of interest, and interpretive signs. And, of course, trails also feel safer and are more interesting when there are other people on them, such as on the well used stair climb in Capitol Hill shown in Figure 2.11.

Following is a detailed description of wayfinding elements that should be considered for the Delridge neighborhood. Several of these activities are excellent opportunities for community input and involvement, public art projects, and marketing contests. A summary of the activities particularly well-suited for community involvement, see section 4.2, Collaboration for Artistic Wayfinding. For an idea on how wayfinding can fit into a neighborhood-wide design idea, see the Urban Design chapter of this report.



Figure 2.11: Capitol Hill Stair Climb

3.2 Directional Signs: Signed Bollards

The main point of having a sign on a bollard (a vertical post two- to three-feet high, *such as the example on the right*) is to guide people to the trail, invite them to walk on public space, to signify connectors in a unified network, and to give pedestrians destination information. Delridge neighborhood signs should include some, if not all, of the following elements:

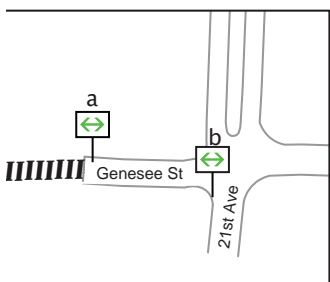


Figure 2.12: Genesee St Sign

- Most importantly, sign placement should make signs clearly visible from the street network. For example, the tops of many of the staircases (e.g., on Andover, Genesee, and Juneau Streets) are set back into dead end rights-of-way, and are not easily visible until the pedestrian is almost literally at the first step. A sign for such a staircase should be placed at the nearest corner (b), rather than at the top of the stairs (a) (Refer to Figure 2.12: Genesee St Sign). Each sign location should be chosen according to the specific attributes of that spot in order to maximize visibility while not interfering with other traffic signage

- Clear labeling
- Walking time to end or intermediate destinations (e.g., 15 minutes to High Point, 10 minutes to Riverview viewpoint)
- Icons and graphics representing destinations and points of interest
- Arrows for directional guidance (e.g., an arrow straight up generally means straight ahead)
















- Staircase signs which include the number of steps (this might particularly draw recreational users to longer staircases)
- Plaques added to signs to indicate sections of trail or staircases that have been adopted by volunteer maintenance crews
- All signs in the neighborhood have similar designs, and are made of similar colors and materials

3.3 Maps and Trail Indicators

Maps are a great way of orienting people to a network of trails. Care should be taken to make maps clear and easy to read. Great maps:

Possible places of interest and icons that could be used on maps

-  Longfellow Creek Legacy Trail
-  Dragonfly Pavilion
-  Fishbone Bridge
-  Duwamish River Trail
-  Alki Trail
-  Riverview Trails
-  Riverview Playfields
-  South Seattle Community College
-  Youngstown Cultural Arts Center
-  Delridge Library
-  High Point
-  Business Sponsors
-  Viewpoints

- Give highest graphic priority to the system they are mapping. Trails, connectors, and stairs (in this case) should have the thickest, most obvious lines
- Include enough landmarks for orientation, but not too many (to avoid being cluttered). In this case, landmarks should include street networks, major street names, and identifiable places
- Have clear, easy-to-read labels and use graphic or icons where possible
- Provide a scale, north arrow, and legend
- Carefully use limited color
- Are updated as the trail system evolves

Hand-held Maps

- Should be easy to print in black and white
- Should be distributed for free
- Should be available in an easy-to-print format on DNDA's website

Stationary Maps

- Are located at key points in the network.
- Have an easy-to-find "You Are Here" orientation dot
- Are in high-traffic areas and made of graffiti-resistant materials
- Can have elements of public art (e.g., the frame, the pedestal, and the icons used on the map for points of interest along trails), which might help deter vandalism⁹
- Are adequately lit for evening use

⁹ The Arboretum in Seattle has specific guidelines about material types, design and dimensions, font type, colors, and locations for signage. See <http://depts.washington.edu/wpa/files/finaliwplan.pdf>

Tall trail markers, bollards, stamped pavement

The Legacy Trail successfully uses brightly colored trail indicators, which allows pedestrians to navigate terrain changes between the street network and earthen trails. East-west connectors (proposed in the section that follows) should also provide a system of trail finders. Wayfinding on these connectors can use a combination of tall trail markers (like the flags for the Legacy Trail), stamped concrete on street corners, and bollards that are decorated but not signed. Stamping concrete with simple designs or a decorative mosaic of recycled materials is an easy and low-effective way to create an inviting pedestrian corridor. Refer to Figure 2.13: Examples of Trail Indicators



Figure 2.13: Examples of Trail Indicators

Entrances to Staircases

The dramatic topography of Delridge is one of the neighborhood's most beautiful natural features. Staircases are uniquely pedestrian structures; highlighting staircase entrances that feed into Delridge Way is a subtle, yet effective way to create a walking-friendly neighborhood. The following enhancements should be considered:

- “Celebrate” the bottoms of staircases on Delridge Way. Beyond regular signage (see “Signs” above), it is common to mark the bottoms of staircases with fountains. For example, note this photo of Haller Fountain and stair climb in Port Townsend¹⁰. To avoid having too many fountains along Delridge Way, the bottoms of many stairs could be “celebrated” with public art
- Widen the bottom steps and clear away even more brush around the bottom, so that the staircases open up onto the sidewalk or street and are more visible



Figure 2.14: Haller Fountain

¹⁰ Wind's Eye Design, Inc. Port Townsend History [WWW page]. URL http://www.ptguide.com/images/history/l_hallerfountain.jpg

from farther away

- Allow for a clear line of sight from both the tops and bottoms of staircases so that once a pedestrian arrives there, she knows where to go next. Note the vegetation clearance along either side of the Haller steps and the clear line of sight



Figure 2.15: Legacy Trail Gateway

Entrances to Trails

Entrances to the Legacy Trail are beautiful (Refer to Figure 2.15: Legacy Trail Gateway), and they incorporate the Seattle Parks and Recreation Dragonfly logo in stamped cement. Entrances to other trails should be celebrated with similarly crafted gateways; the Dragonfly cement stamp should be used at any entrance to Seattle Parks and Recreation land.

3.4 General Trail Recommendations

Trail and Staircase Maintenance

In order to feel safe and inviting, trails and staircases should be maintained regularly:

- Clear away dense brush and only allow low underbrush to grow within 5 to 10 feet of either side of the trail or staircase. This will help open up the staircases to more light and visibility, with the intent of improving safety
- Where possible, strategically clear enough brush so that the trail or staircase can be monitored by nearby homes (Refer to Figure 2.16: Line of Sight from Homes). Particularly on steep slopes, this can be done while maintaining privacy for homeowners by creating sight lines from the homes onto lower parts of the trail. This will not be possible for all trails, such as along the Riverview Trail and the staircase on the Graham St. right-of-way leading up to High Point, both of which go through dense greenbelts



Figure 2.16: Line of Sight from homes

Trail Amenities

The following amenities should be considered at regular or semi-regular intervals along trail networks in Delridge:

- Functional, consistent, and closely spaced lighting
- Benches
- Garbage cans
- “Doggie stations” with plastic bags for cleaning up after pets
- Picnic tables (particularly at nice viewpoints, such as at the Riverview Playfields, or even at staircase landings)
- Interpretive signs
- Trail-side or stair-side gardens or P-Patches (such as the one along the Legacy Trail, or this picture from Capitol Hill, *Figure 2.17*)



Figure 2.17: Garden Stairs in Capitol Hill

Neighborhood Unification: The Legacy Trail as a Standard

The Legacy Trail is already a good example of a functional wayfinding system for this neighborhood, where routes alternate between wooded, more “natural” trails, street networks, street crossings, and stairs. There are a few options to consider for more wayfinding as the trail network in the neighborhood continues to expand:

- Use the Legacy Trail wayfinding system as the neighborhood standard so that all other trails use the same materials, colors, and systems. The Legacy Trail will cease to be unique, but will become the central backbone in a unified network of trails
- Set wayfinding standards for the rest of the network which are similar to the Legacy Trail, but not the same. This could mean that other systems use the same materials (e.g., galvanized steel, mosaics, dragon stamps, and flags), but with different graphics, logos, and designs

Overall Effect of Network Signs and Wayfinding on Pedestrian Experience

Consistent signage and wayfinding strategies can change the perception a pedestrian has of a particular place. The following figure (*Figure 2.18: Mind Map of Network*), illustrates the expanded perception a pedestrian can have of nearby destinations, even if they are not directly visible.

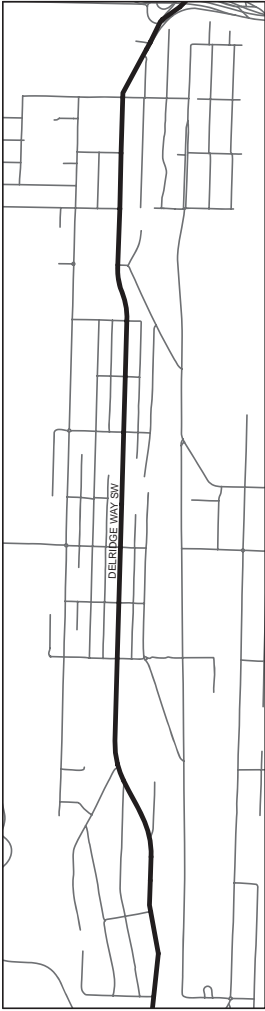


Figure a: Existing Street Network

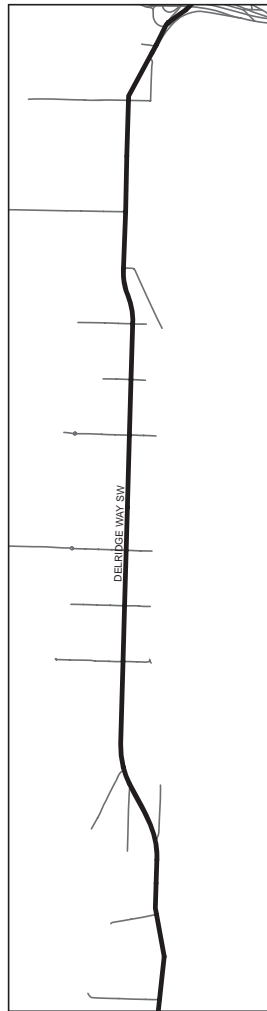


Figure b: Perceived Pedestrian Network from Delridge Way

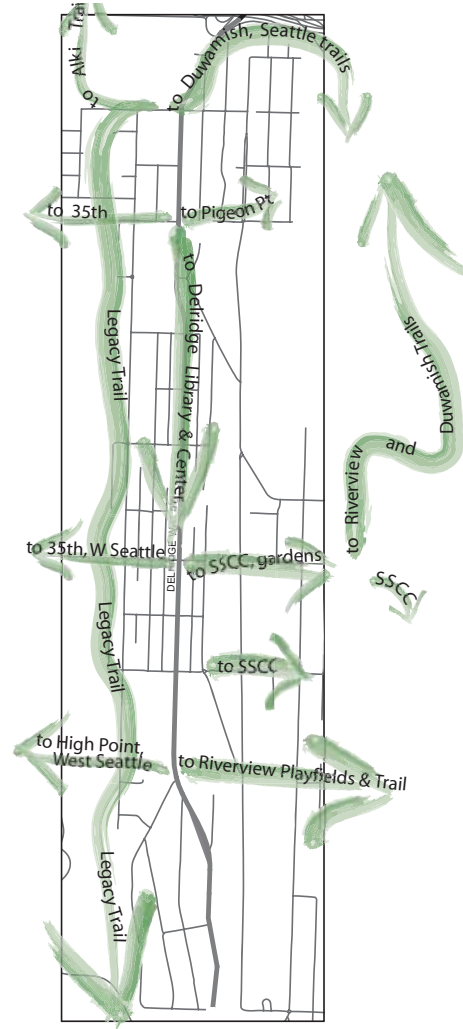


Figure c: Perceived Pedestrian Network from Delridge Way with signs and wayfinding

The existing street network in the Delridge Neighborhood is shaped by the steep topography on either side of Delridge Way. Note, Figure a, how the grid is incomplete in places, primarily due to steep slopes.

A pedestrian walking down Delridge Way who is unfamiliar with the streets will perceive only a network that he or she can see from the main road. The pedestrian does not know where streets lead to when they are out of his or her line of sight. Therefore, the perceived pedestrian network is a truncated version of the street network, as in Figure b. Without physically exploring each one, and without a detailed map, the pedestrian is unaware of other destinations that might be near by.

Figure c shows the network that a pedestrian might perceive if signs and wayfinding strategies are present along Delridge Way. The pedestrian may not know the exact trail, streets or staircases yet, but he or she will have a sense of being connected to a variety of other destinations via the proposed trails network.

Figure 2.18: Mind Map of Network

General Drainage Recommendations

Many of the proposed and existing trail design recommendations include paths and staircases on steep slopes (Refer to Figure 2.19: Drainage Chart). Adequate drainage in these areas is critical to prevent erosion and runoff, as well as to help protect the structural integrity of the trail or staircase. Permeable trail surfaces, such as crushed gravel, dirt, or permeable paving allows water to filter through into the ground, thereby reducing erosion and minimizing water damage to the trail over time. Permeable surfaces also help the trail dry out and prevent water pooling after heavy precipitation.

The Drainage Structure Order of Operations	
Outslope the trail	More time consuming, but the best solution.
Drain Dip	Drain dips are simple and effective, but must be properly built and routinely maintained if they are to last.
Waterbars	More elaborate structures that require considerable time and materials to build, but are often necessary in heavily used, steep, or very wet trails.
Culverts	Use in heavily trafficked trails in conjunction with side ditches and turnpikes.
Check dams	Used only in badly eroded, trenched trails. They don't remove water from the trail, but stop further erosion

Figure 2.19: Drainage Chart

Drainage options include¹¹:

- Drainage dips or grade dips: A drain dip or grade dip is a wide, shallow depression sculpted into the trail (Refer to Figure 2.20)
- Waterbars: Waterbars are a more sophisticated version of drain dips that are reinforced with either rock or a log to help sustain a greater volume of water. Rock waterbars last much longer than wood waterbars, but take longer to make and require a long search for suitable rocks. (Refer to Figure 2.21)
- Culverts: When large volumes of water need to be moved across the trail at a single spot, culverts are used. (Refer to Figure 2.22)



Figure 2.20: Drainage Dips



Figure 2.21: Waterbars



Figure 2.22: Culverts

11 Washington Trails Association. (2005). Drainage Structures [WWW page]. <http://www.wta.org/~wta/cgi-bin/wtaweb.pl?6+tw+5>. Note: For installation instructions refer to the Washington Trails Association directions (<http://www.wta.org/~wta/cgi-bin/wtaweb.pl?6+tw+5>)

- Check dams: When a trail becomes so badly eroded that it has turned into a deep trench, it can be difficult to divert the water off the trail. Check dams (also known as check steps) can be made by building a sturdy wall from large rocks or wood bars across the trail
- Vegetation: Short plantings along the perimeter of the trail will help prevent erosion and runoff

3.5 Specific Corridor Recommendations

In and surrounding the Delridge neighborhood, there is great potential for connecting existing formal and informal trails. Following are recommendations for creating a connected trail system that incorporates the existing street network, staircases, and trails. *Refer to Figure 2.23: Map of Prioritized Routes.*

THE DUWAMISH – LEGACY CONNECTOR

The City of Seattle and the Port of Seattle are partnering to extend the Duwamish Trail from Idaho St. to Chelan Ave., underneath the West Seattle Bridge. This project will complete the missing link between the Duwamish and Alki Trails and is scheduled to begin the summer of 2006. This extension will bring the Duwamish Trail within 0.7 miles of the Legacy Trail's north entrance (*Refer to Figure 2.31*). Thus, there is an exciting opportunity to link the Duwamish and Legacy Trails along the existing street network with wayfinding tools. The following discussion explores this opportunity, assuming that the Duwamish Trail is extended as planned.

Recommended Route

At Marginal Place and 17th Ave., the Duwamish Trail departs West Marginal Way, heading west along Marginal Place. After one block, the trail merges with the foot and bike path under the West Seattle Bridge. The Duwamish-Legacy connector would follow the southernmost path under the West Seattle Bridge to Delridge Way. Continuing one-and-a-half blocks south on Delridge Way, the trail would cross at Andover St., then head west through three blocks of commercial and residential streets to the north entrance of the Legacy Trail at Yancy St. (*Refer to Figure 2.24: Map of Duwamish-Legacy Connector*) Currently, the connector is challenging to navigate due to a lack of wayfinding mechanisms. This is a perfect time to coordinate wayfinding mechanisms with SDOT partners, as they begin the Duwamish Trail extension.

This connector poses a challenge for southbound bikers. As bikers emerge from a wide, two-way path under the West Seattle Bridge they end up on a narrow sidewalk along Delridge Way, going against traffic for one-and-a-half blocks. An alternative means should be explored to assist bikers.

(This page is a placeholder for Figure 2.23: Map of Prioritized Routes)

(This page is a placeholder for Figure 2.23: Map of Prioritized Routes

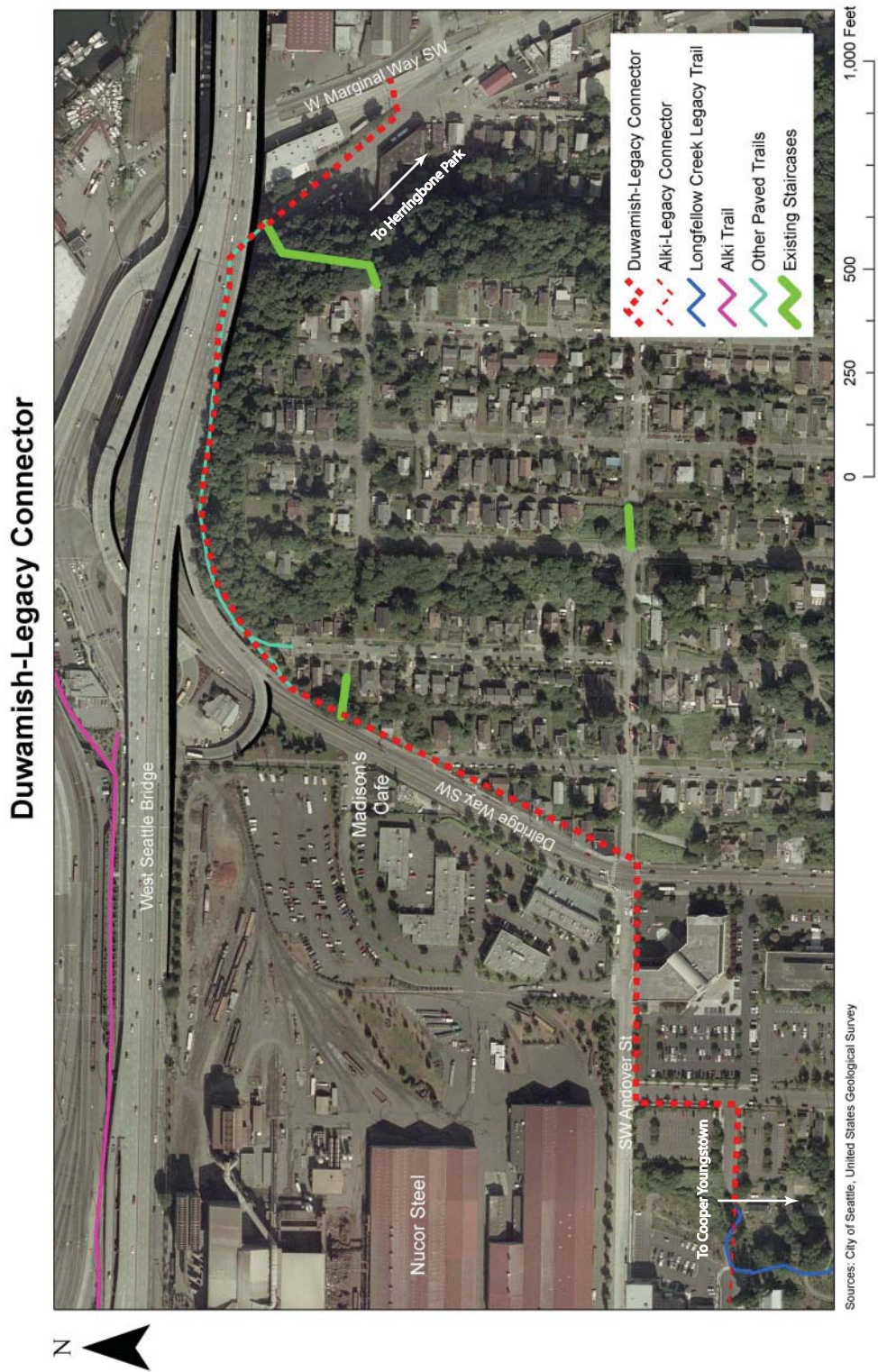


Figure 2.24: Map of Duwamish-Legacy Connector

Wayfinding Recommendations

- Map:** Post map on the east side of West Marginal Way. On the map, indicate where the trail leads and provide distances to various locations. The adjacent figure (Figure 2.25: Example Stationary Map) gives a flavor for components that could be included in a wayfinding map. Partnering with interested businesses to include them on the map could result in monetary or in-kind support. In addition, the map pedestal could be designed by a community artist (Refer to Figure 2.26: Pedestal)¹². See the General Recommendations section for more detailed map recommendations and see sample map. (Figure 2.46)
- Tall Markers:** Place tall trail markers (similar to those marking the Legacy Trail) at the five locations indicated on the Duwamish-Legacy Connection Map (Figure 2.24)
- Bollards/Stamped Pavement:** Space brightly colored bollards where pedestrians have multiple options for routes. Tall trail markers or stamped pavement can be used in combination with bollards
- Sign:** Place a sign at the fork in the trail under West Seattle Bridge indicating Legacy Trail and Delridge neighborhood to the left and Alki Trail to the right. Include both words and symbols commonly associated with the trails. See the General Recommendations section for more detailed recommendations
- Gateway:** Locate a gate entrance on the connector, as it emerges from under West Seattle Bridge and into the Delridge neighborhood. The gateways help to define and celebrate the neighborhood. Refer to Figures 2.27 and 2.28

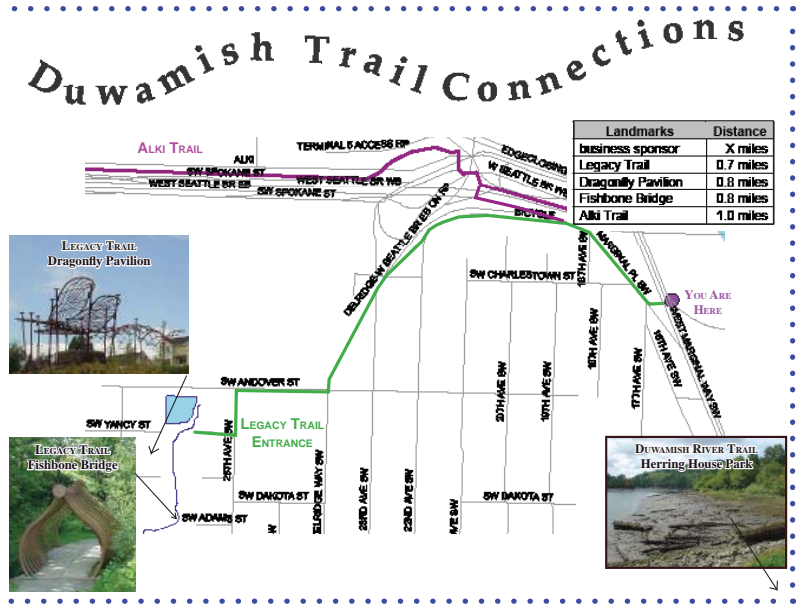


Figure 2.26: Pedestal

12 The Seattle Arboretu: <http://depts.washington.edu/wpa/files/finaliwplan.pdf>



Figure 2.27: Existing Gateway

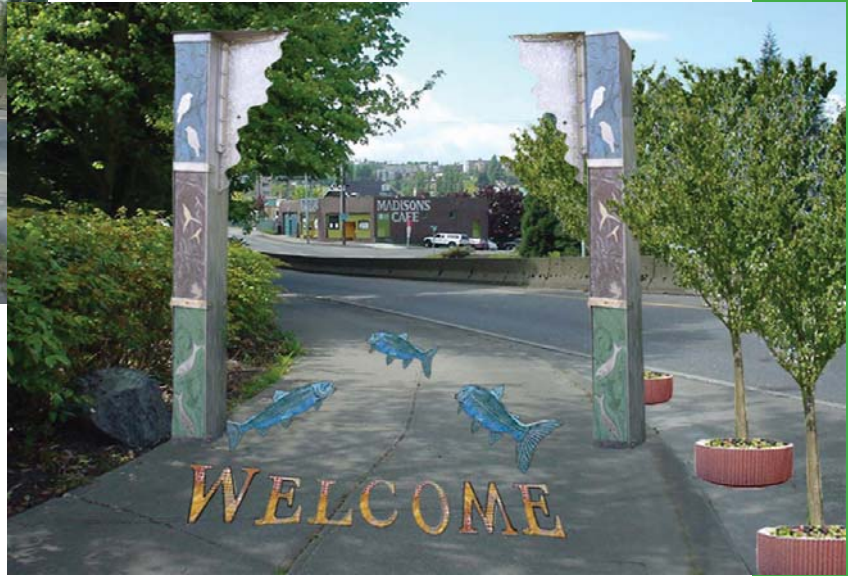


Figure 2.28: Proposed Gateway



Figure 2.29: Existing Duwamish-Legacy Connector



Figure 2.30: Proposed Duwamish-Legacy Connector

Safety Recommendations

West Marginal Way, Marginal Place and 17th Ave. Intersection

The Duwamish Trail extension will continue on the east side of West Marginal Way from Idaho St to 17th Ave. At 17th Ave. it will turn west, crossing West Marginal Way, a thoroughfare with heavy, fast-moving freight and car traffic. There is currently a stoplight at the intersection of West Marginal Way and 17th Ave, but additional safety measures should be taken to make this crossing safer for trail users.

- **Crosswalk:** Paint the street in bright white and yellow to establish a pedestrian- and bike-friendly crossing and to alert vehicles
- **Pedestrian and bike signs:** Post signs to alert northbound and southbound vehicles of upcoming trail crossing
- **Flashing lights:** Install for northbound and southbound traffic to alert of upcoming trail crossing

Under West Seattle Bridge

This is a direct, traffic-free connection from West Marginal Way to Delridge Way. The connection under the bridge is lighted and well-maintained with vegetation lining the wide path. However, it is also secluded and would benefit from additional investments. *Refer to Figures 2.29 and 2.30*

- **Murals:** Paint murals on concrete posts and ramps (acquire city approval)
- **Public Art:** Include in space under bridge
- **Trail Connections:** Well-marked connections to the Legacy, Alki, and Duwamish Trails will create more pedestrian and bike traffic (there is safety in numbers)

Duwamish-Legacy Connector Summary

Also Refer to end of chapter for additional Summary data.

<u>Duwamish Connection</u>	
Landmarks	Distance
business sponsor	X miles
Legacy Trail	0.7 miles
Dragonfly Pavilion	0.8 miles
Fishbone Bridge	0.8 miles
Alki Trail	0.5 miles

<u>Duwamish Improvements</u>
Wayfinding
1 Map
5 Tall Trail Markers
1 Sign (metal)
1 Gateway entrance
Stamped Pavement
Safety
Street painting and murals
4 Pedestrian/ bike signs
2 Flashing lights
Public Art

THE ALKI – LEGACY CONNECTOR

The Alki Trail is a paved, eight mile path that wraps around the north horn and western side of the West Seattle peninsula. Although construction to link the Alki and Duwamish Trails will begin the summer of 2006, it is still advisable that two separate connections exist to the Legacy Trail: one from the Duwamish Trail (at the east end of the West Seattle Bridge) and the other from the Alki Trail (at the west end of the West Seattle Bridge).



Figure 2.31: Connecting the Alki, Legacy, and Duwamish Trails

Providing two separate connections that lead trail users from both the Duwamish and the Alki Trails into the Delridge community and to the Legacy Trail will improve overall trail connectivity in the area. The Alki and Legacy Trails can easily be linked via the existing street network by means of some simple wayfinding mechanisms.

There is only a half-mile disconnect between the two trails.

Recommended Route

The Alki Trail reaches the West Seattle Bridge via Harbor Ave. The proposed connection route passes under the bridge, continuing south on Avalon Way (previously Harbor Ave.) for a quarter mile. The connection turns east on Yancy St. and dead-ends at the north entrance to the Legacy Trail and the proposed Duwamish – Legacy connector. Refer to Figure 2.32: Map of Alki - Legacy Connector

Wayfinding Recommendations

- **Map:** Post map along Alki Trail on Harbor Ave., just north of the West Seattle Bridge. Indicate where the trail leads and the distance to each location
- **Tall Markers:** Place tall trail markers (similar to those marking the Legacy Trail) at the three locations indicated on the Alki – Legacy Connection Map above
- **Bollards/ Stamped pavement:** Place brightly colored bollards as needed
- **Gateway:** Gate entrance on Avalon Way just south of West Seattle Bridge (similar to Legacy Trail gateways)

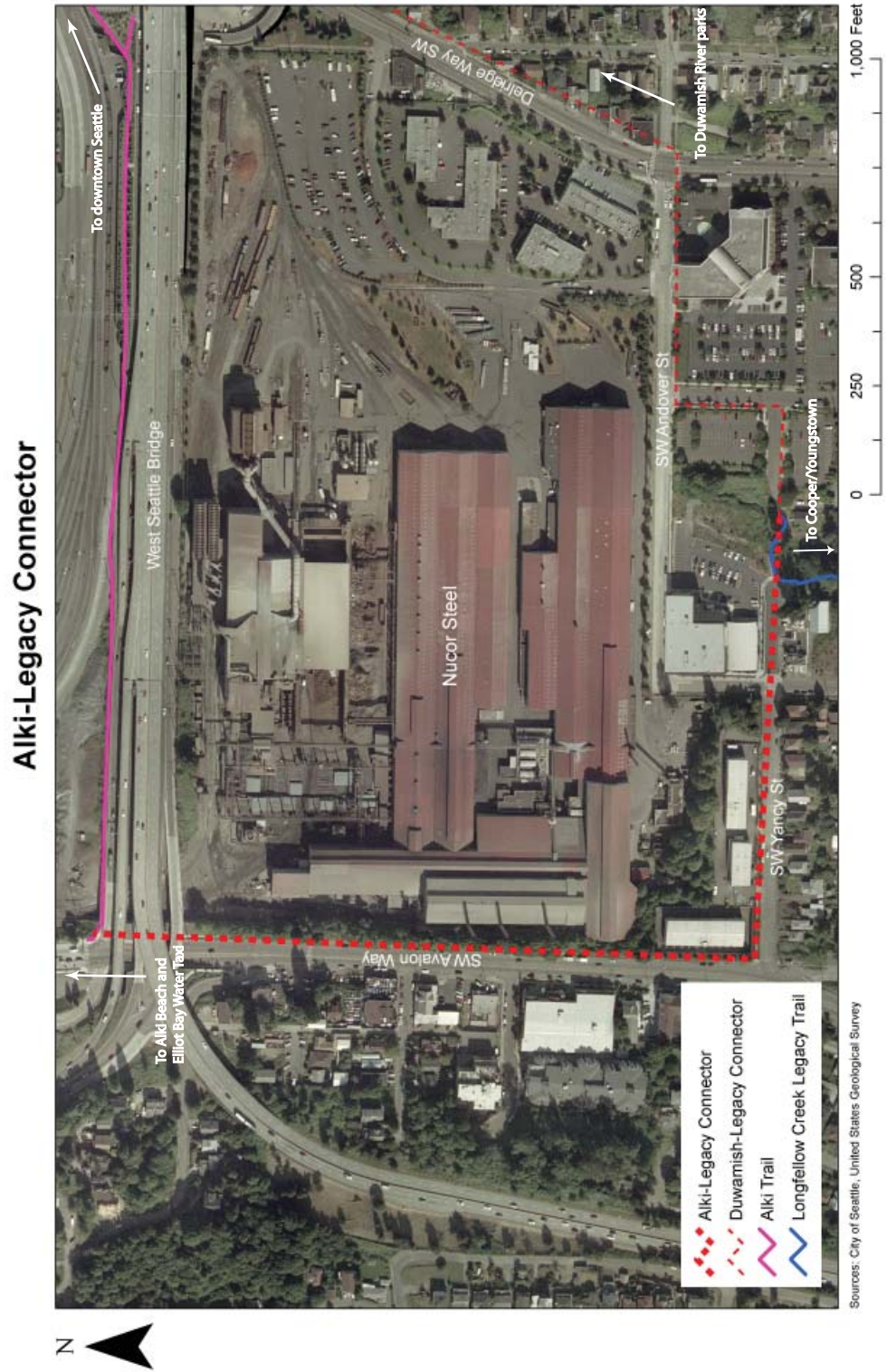


Figure 2.32: Map of the Alki-Legacy Connector

Safety Recommendations:

Harbor Ave. and West Seattle Bridge Ramps

The intersection of Harbor Ave. and the West Seattle Bridge off- and on-ramps is regulated by traffic signals and pedestrian crosswalks. Simple street paintings and signs might enhance vehicle awareness of the trail crossing.

- **Crosswalk:** Paint the street in bright white and yellow to establish a pedestrian- and bike-friendly crossing and alert vehicles
- **Pedestrian and bike signs:** Post signs to alert vehicles of upcoming trail crossing

<u>Alki Connection</u>	
Landmarks	Distance
business sponsor	X miles
Legacy Trail	0.5 miles
Dragonfly Pavilion	0.6 miles
Fishbone Bridge	0.6 miles
Duwamish River Trailhead	0.5 miles

<u>Alki Improvements</u>	
Wayfinding	
1 Map	
4 Tall Trail Markers	
1 Gateway entrance	
Safety	
Street painting and murals	
2 Pedestrian/ bike signs	

Alki-Legacy Connector Summary

Also Refer to end of chapter for additional Summary data.

Wayfinding Recommendations

THE ALKI – DUWAMISH CONNECTOR

In addition to the above recommendations, there should also be wayfinding mechanisms installed at the north exit of the Legacy Trail directing trail users to the Alki and Duwamish Trails.

- **Map:** Post a map at the north exit of the Legacy Trail on Yancy Street. Indicate where the trail leads and the distance to each location
- **Bollard:** Add directions to the Alki and Duwamish Trails to the existing north Legacy Trail bollard

<u>Legacy Connection</u>	
Landmarks	Distance
business sponsor	X miles
Alki Trail	0.5 miles
Duwamish Trail	0.6 miles

<u>Legacy Improvements</u>	
Wayfinding	
1 Map	
Additional plaques added to existing bollard	

THE GENESEE CONNECTOR

North end of Legacy Trail, Summary of Connection to Alki and Duwamish Trails

Genesee St. provides an important east-west connection, passing through the north end of the Delridge neighborhood. The combined street and stair connection provides access to Cooper Elementary, the Youngstown Cultural Arts Center, the Legacy Trail, the West Seattle Golf Course, Camp Long, and the eastern portion of West Seattle. At Delridge Way there is a traffic signal with marked crosswalks allowing for safe pedestrian travel along Genesee St. This connector is completed and useable in its current state. A couple of minor modifications and wayfinding mechanisms would make it a safer and more comfortable route. Refer to Figure 2.33: Map of Genesee Connector



Wayfinding Recommendations

- **Signed Bollards:** Install bollards at each landmark along the Genesee route that indicate where the trail leads and the distance to other locations
- **Sign:** Install a small sign at the bottom of the Genesee staircase, indicating the number of stairs to the top
- **Entrance:** Celebrate the stair entrance with public art or other visible marker

Safety, Maintenance and Comfort

- **Thin Vegetation:** Thin the vegetation around the stairs to provide more visibility, providing a sight line to nearby homes whenever possible
- **Clear Moss:** Scrape moss off of the stairs, both for pedestrian safety and to protect the structural integrity of the staircases
- **Benches:** Improve or add benches at the staircase landings on 22nd and 21st Avenues
- **Trash Cans:** Provide trash receptacles on 22nd Ave. and Delridge Way

Genesee Connection					
Locate Bollards Here	Distances (Miles) to these locations				
	Cooper Elementary School	Youngstown Cultural Art Center	Legacy Trail	West Seattle Golf Course	Camp Long
21st Ave	0.2	0.3	0.5	0.6	1
Delridge Way	0.3	0.1	0.3	0.4	0.8
26th Ave	0.4	0.2	0.1	0.1	0.5
Avalon Way	0.8	0.5	0.3	0.2	0.2

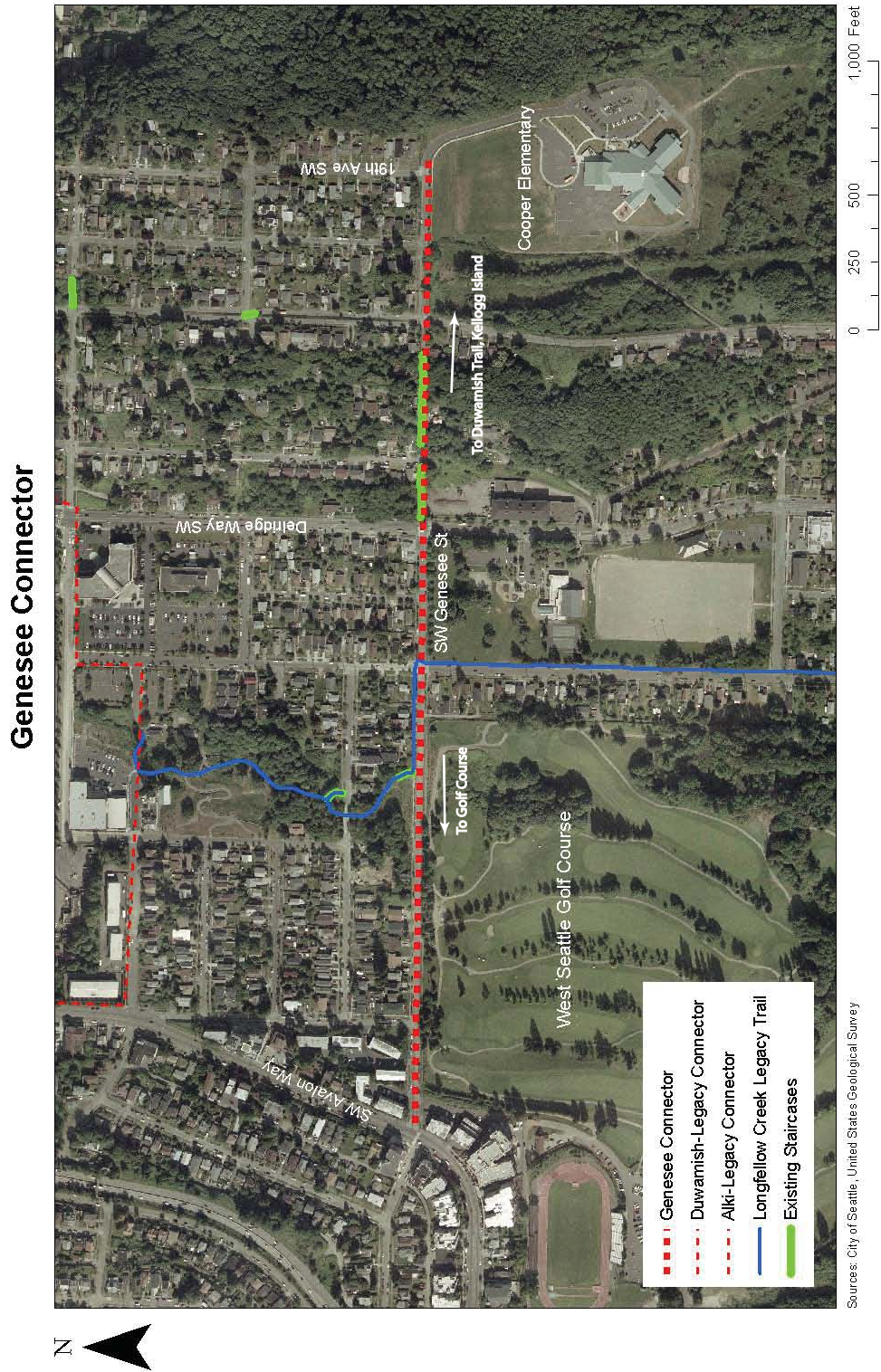


Figure 2.33: Map of Genesee Connector

THE BRANDON CONNECTOR

Genesee Connector Summary

Also Refer to end of chapter for additional Summary data.

Brandon St. runs right through the center of the Delridge neighborhood and is an obvious east-west corridor. Refer to Figure 2.34: Map of Brandon Connector. This street provides access to several focal points in the community, including the Legacy Trail and the Brandon Playfield Park, which are just two blocks west of the Delridge Way and Brandon St. intersection. The new public library and DNDA are also situated at this key intersection.

The Brandon right-of-way continues west past the entrance to the Legacy Trail to the southern end of the West Seattle Golf Course and an entrance to Camp Long. This western stretch of Brandon St., where it meets up with Camp Long and the golf course, seems to dead end as the trail changes from sidewalk to a narrow gravel path. The continuation of an informal social trail is not particularly inviting for pedestrians, nor does it appear to be a “through route” to the west end of the neighborhood. Similarly, the eastern stretch of Brandon St. dead ends at 23rd Ave, with a steep and thickly vegetated hill climb up to 21st Ave.

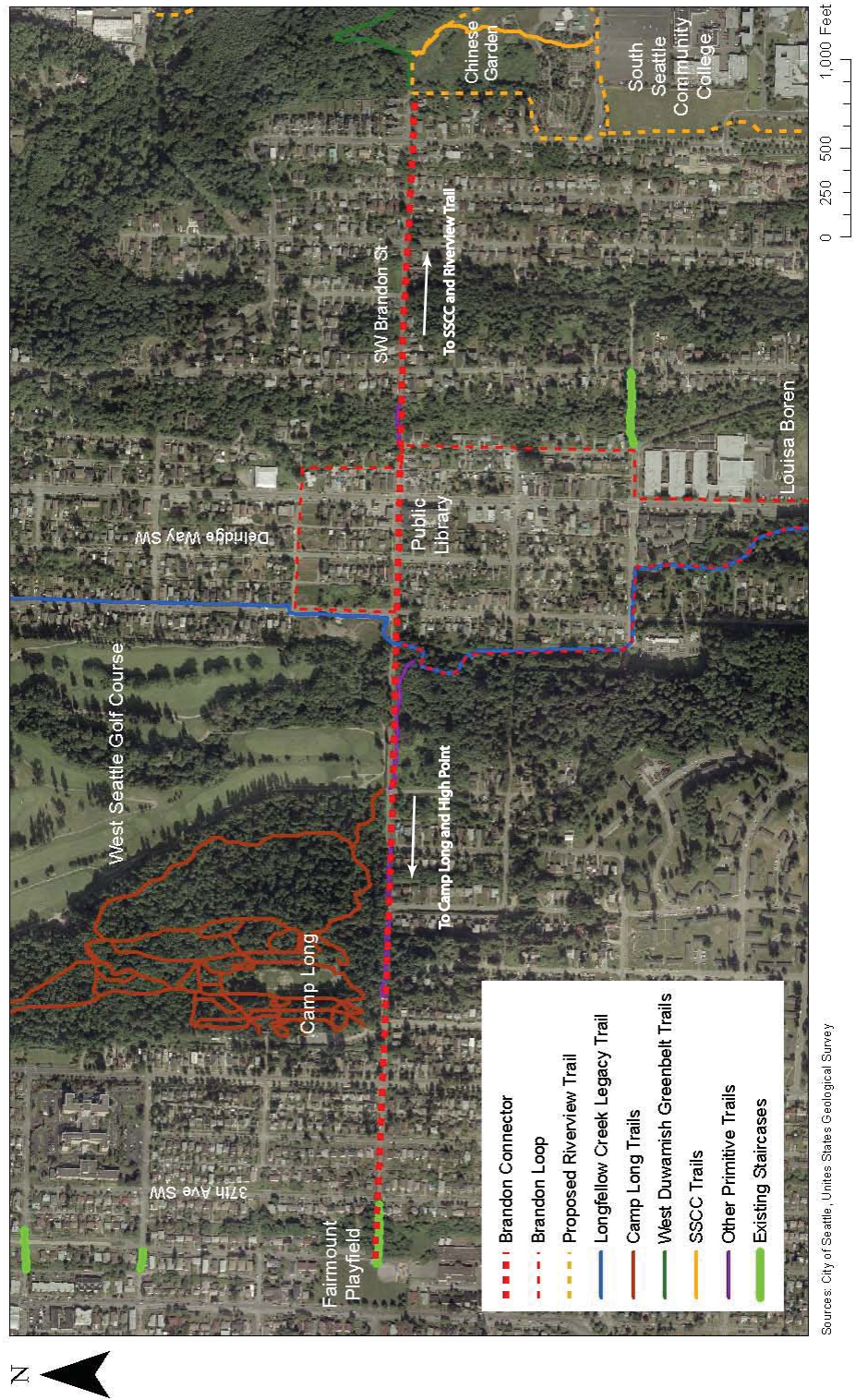
The intersection of Brandon and Delridge is the focal point of many community facilities and proposed commercial development. Because of its location, extending Brandon to the east and west ridges will be a valuable improvement to trail connectivity and pedestrian accessibility. Brandon is also an ideal corridor to connect the Legacy Trail to Camp Long on the western edge of the neighborhood, as well as to the Riverview Trail and SSCC on the eastern ridge. This proposed extension has three elements: West Extension, East Extension, and Continuation to the Duwamish Greenbelt and Kellogg Island via the Riverview Trail.

General Wayfinding Recommendations

- **Signed Bollards:** Install signed bollards in the following locations:
 - o Brandon Street and 35th Ave.
 - o Brandon Street 26th Ave.
 - o Brandon Street Delridge Way
 - o Brandon Street 21st Ave.
 - o Brandon Street 17th Ave.
 - o Brandon Street and 16th Ave.



Brandon Connector



Sources: City of Seattle, United States Geological Survey

Figure 2.34: Map of Brandon Connector

- Destination points to include on bollards: Camp Long, West Seattle Golf Course, Longfellow Creek Legacy Trail, DNDA, library, SSCC, Chinese Gardens, Riverview Trail, Kellogg Island

Safety, Maintenance and Comfort – West and East Extensions

- **Handrails:** Install handrails along both sides of the proposed trails to assist pedestrians with the slope of the hill climb
- **Lighting:** Install ample lighting on the north side of the trails. This will help increase night visibility along the heavily vegetated corridor
- **Thin Vegetation:** Thin out thick foliage bordering the trails to increase visibility and prevent shrubbery from encroaching on the trails
 - **Trail Width:** Increase trail width to approximately six feet
 - **Clearing Height:** Maintain trail clearing height to minimum nine feet



Figure 2.35: Existing Camp Long entrance & Brandon St

West Extension Recommendations

Improve the existing informal path from the southern entrance of Camp Long to 35th Ave to create a pedestrian friendly trail. Refer to Figure 2.35: Existing Camp Long entrance & Brandon St and Figure 2.36: Proposed Camp Long entrance & Brandon St.



Figure 2.36: Proposed Camp Long entrance & Brandon St

Safety, Maintenance and Comfort

- **Trash Cans:** Provide a trash can at Brandon and 35th
- **Trail Surface:** Resurface path with crushed gravel or recycled asphalt products
- **Bench:** Provide a bench at Brandon and 26th, as this makes the entrance to the trail more welcoming and offers a place to stop and rest

East Extension Recommendations

Add a staircase to connect 23rd Ave. and 21st Ave. and add a staircase linking 17th and 16th Avenues. This will connect the Legacy Trail to SSCC and the Riverview Trail, which will become an increasingly important as the Brandon Node develops. Refer to *Figure 2.37: Existing Brandon St hill climb* and *Figure 2.38: Proposed Brandon St hill climb*

Safety, Maintenance and Comfort – East Extension

- **Bench and Landing:** Install a landing with a bench at the top of the staircase on 21st Ave. This will provide a nice resting spot and viewpoint between the two hill climbs
- **Trash Cans:** Provide a trash can at the top of the 21st St. landing
- **Trail Materials:**
 - o Option 1: Cement stairs consistent with others in the neighborhood
 - o Option 2 (preferred): Consider installing woodbar and dirt or gravel steps that offer a natural feel and may help to integrate the Delridge trail system more successfully than cement stairs

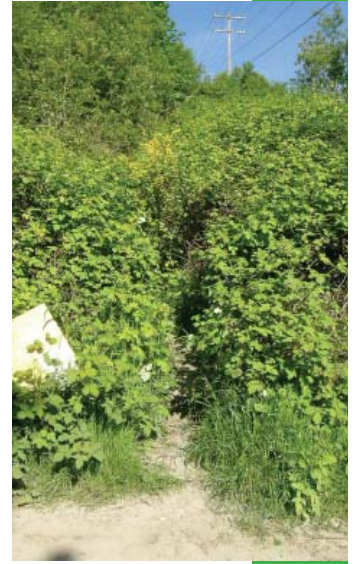


Figure 2.37: Existing Brandon St hill

Continuation to Duwamish Greenbelt and Kellogg Island via Riverview Trail

There is an opportunity for trail users to link up with the Riverview Trail, both at the intersection of Brandon St. and 16th Ave. or at Juneau St. and 16th Ave. Following Brandon St. across 16th Ave. there is an entrance to the Riverview Trail, just north of the SSCC parking lot and Chinese Gardens. Likewise, taking Juneau across 16th leads directly into the Chinese Gardens at SSCC, a gorgeous spot to visit before continuing on to the Riverview Trail. Refer to *Figure 2.39: Chinese Gardens at SSCC*. A formal path connecting the Chinese Garden to the Riverview Trail is not yet complete; however rTrip will be finishing the trail in the near future.



Figure 2.38: Proposed Brandon St hill climb

Once on the Riverview Trail, trail users have a connection through the West Duwamish Greenbelt to Kellogg Island. Currently, the Riverview Trail is fairly primitive with an average width of about two or three feet that traverses through a heavily vegetated area. Steep slopes, muddy terrain, and lack of lighting are just a few of the issues preventing the Riverview Trail from being a central pedestrian corridor. A number of improvements are necessary for the Riverview Trail to be successful.



Figure 2.39: Chinese Gardens at SSCC

Safety, Maintenance and Comfort

- **Trash Can:** Provide a trash can at the easternmost end of the trail, where the trail meets up with West Marginal Way
- **Trail Surface:** Resurface path with crushed gravel or recycled asphalt products

Brandon Loop

This loop presents a unique opportunity to activate central Delridge while introducing users to the varied paths that pass through the neighborhood. The proposed loop would extend from Puget Boulevard to Graham Street, traveling along the Legacy Trail, Delridge Way sidewalks, and residential streets, passing the well-defined Brandon and Graham connectors. *Refer to Map 2.40: Brandon Loop, see Also Urban Design Chapter*

Brandon Connector Summary

Also Refer to end of chapter for additional Summary data.

<u>Brandon Street Connection</u>		
Destination	Start Point	Distance
Camp Long (South Entrance)	36th Ave SW	0.9 miles
Legacy Trail	Camp Long S. Entrance	0.4 miles
SSCC/rTrip Trail	Legacy Trail	0.5 miles

THE GRAHAM CONNECTOR

Currently, Graham St. is a completed and usable connector linking High Point on the neighborhood's western ridge to SSCC on the eastern ridge. It is an exciting connector through the middle of Delridge, traversing through a beautiful urban greenbelt just west of Delridge Way, linking up with the Legacy Trail, and holding one of the most impressive stretches of staircase in the neighborhood. *Refer to Figure 2.41: Map of the Graham Connector.* The three primary obstacles to making Graham St. an inviting gateway to Delridge, High Point, and SSCC are: a trail barrier at an unused Louisa Boren School playfield, scant wayfinding mechanisms, and a lack of staircase maintenance.

Designating a direct right-of-way and installing signage, such as wooden bollards and stamped concrete, will help pedestrians navigate the segmented components of the Graham St. connection. SDOT is also planning to install a crosswalk at the intersection of Graham St. and Delridge Way, which is critical to the accessibility of this east-west link.





Figure 2.40: Brandon Loop

Graham Connector



Figure 2.41: Map of the Graham Connector

Graham and Louisa Boren School Field

One of the primary barriers preventing Graham St. from being a successful trail connector and east-west linkage is the barrier at the Louisa Boren School field. Pedestrians heading east on Graham St, after crossing Delridge Way are met by an apparent dead end at the Louisa Boren field. There is no clear pedestrian path beyond the chain-link fence and across the field to the Graham Street stairs to the east.

The staircase descending the eastern slope ends with a view of a metal storage shed and no formal path through the Louisa Boren playfield. Refer to Figure 2.42: Existing Trail at Louisa Boren field. DNDA has developed an agreement with the school district to allow a continuation of the Graham Connector through a portion of the field. A formal path and attractive feature, such as a mosaic courtyard or fountain, will help to celebrate this gateway to the Brandon Node. Refer to Figure 2.43: Proposed trail at Louisa Boren field



Figure 2.42: Existing Trail at Louisa Boren Field

Wayfinding Recommendations for Louisa Boren Field

- Move the metal storage shed at the base of the Graham stairs at the eastern edge of the Louisa Boren field
- Create a pathway between the two current ballfields and open a pedestrian passage through the chain-link fence to the pending crosswalk on Delridge Way
- Install bollards in the following locations:
 - o Bottom of the Graham staircase and Louisa Boren field
 - o Intersection of Delridge Way and the path through Louisa Boren fields
- Consider installing a fountain, courtyard, or other feature at the bottom of the Graham stairs



Figure 2.43: Proposed Trail at Louisa Boren Field

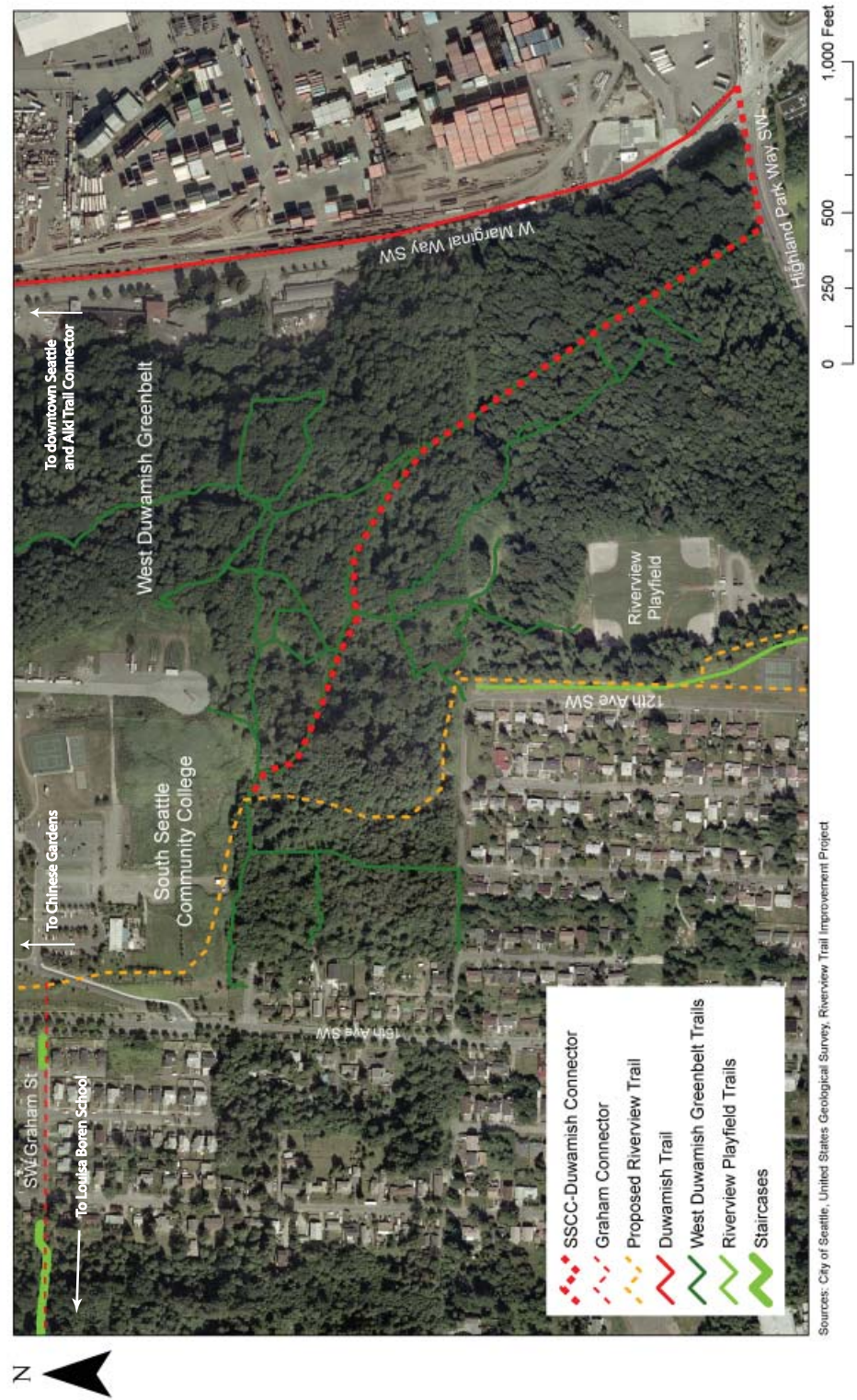
Safety, Maintenance and Comfort

- Plant small plantings and shrubs along the proposed trail through the field
- Install four-foot high bollard lights from the bottom of the staircase and along the trail around Louisa Boren field
- Trail Materials – Crushed gravel
- Trail Width – Approximately four feet

The SSCC – Duwamish Connector

After traversing Graham St. from west to east, the very top of the staircase ends at the south side of 16th Ave., just in front of SSCC. However, in order to enter the campus, the traveler is

SSCC-Duwamish Connector



Sources: City of Seattle, United States Geological Survey, Riverview Trail Improvement Project

Figure 2.44: Map of SSCC-Duwamish Connector

forced to walk for a few minutes in either direction to the nearest crosswalk. Other pedestrians may choose another, less safe option: crossing 16th Ave. and dodging two lanes of oncoming traffic.

The installation of a pedestrian crosswalk and light will provide a safe crossing and slow-down traffic for students and pedestrians walking along 16th. This improvement will also make Graham St. a primary, user-friendly connection from Delridge to SSCC and the proposed Riverview Trail. *Refer to Figure 2.44: Map of SSCC-Duwamish Connector*

Wayfinding for all of Graham Connector

- **Bollards:** Install signed bollards in the following locations:
 - o Graham St. and 35th Ave.
 - o Graham St. and Delridge Way (SDOT to install crosswalk and stop light)
 - o Graham St. and 16th Ave.
 - o Install brightly colored or decorative bollards along the trail, particularly at junctions with informal or “social” trails
- **Stamped Concrete, Mosaic Sidewalks, and Crosswalks:** Stamp concrete with simple designs or a decorative mosaic of recycled materials. The repetition of place-making tools at various intersections along the trail provides continuity to the journey. These techniques are recommended in the following locations:
 - o Graham St. and 35th Ave.
 - o Graham St. and Delridge Way
 - o Graham St. and 16th Ave.

Safety, Maintenance, and Comfort

While the majority of Graham staircases are in fair or good condition, almost all would benefit from improvements. Lighting along the staircases is inadequate for night travel, and heavy brush and vegetation along the stairs is a safety concern. Many of the benches at staircase landings are in disrepair or have been partially removed. Lastly, many of the staircases were extensively littered with garbage, suggesting a need for more trash cans.

- **Clear Moss:** Scrape moss off of the stairs, both for pedestrian safety and to protect the structural integrity of the staircases
- **Maintenance:** Repair chipped edges and cracks in staircases
- **Thin Vegetation:** Thin out foliage around stairs to provide more visibility, providing a sight line to nearby homes whenever possible
- **Benches:** Improve or replace benches at staircase landings

Summary of Graham Connector

Also Refer to end of chapter for additional Summary data.

Genesee Connection					
Locate Bollards Here	Distances (Miles) to these locations				
	Cooper Elementary School	Youngstown Cultural Art Center	Legacy Trail	West Seattle Golf Course	Camp Long
21st Ave	0.2	0.3	0.5	0.6	1
Delridge Way	0.3	0.1	0.3	0.4	0.8
26th Ave	0.4	0.2	0.1	0.1	0.5
Avalon Way	0.8	0.5	0.3	0.2	0.2

THE MYRTLE CONNECTOR

Like Graham St., Myrtle St. is also connects the Legacy Trail in the west to the Riverview Playfield and the Riverview Trail on the eastern ridge. However, this corridor needs better wayfinding and general improvements to vegetation maintenance. Refer to Figure 2.45: Map of Myrtle Connector

Wayfinding

- **Bollards:** Install bollards in the following locations:
 - Myrtle St. and Sylvan Way
 - Myrtle St. and Delridge Way
 - Myrtle St. and 21st Ave.
 - Myrtle St. and 16th Ave.
 - Myrtle St. and 12th Ave.
- **Stamped Concrete and Mosaics :** These techniques should be used in the following locations:
 - Myrtle St. and Delridge Way
 - Destination Points along Myrtle St. include the Legacy Trail, Riverview Playfield, and the Riverview Trail.

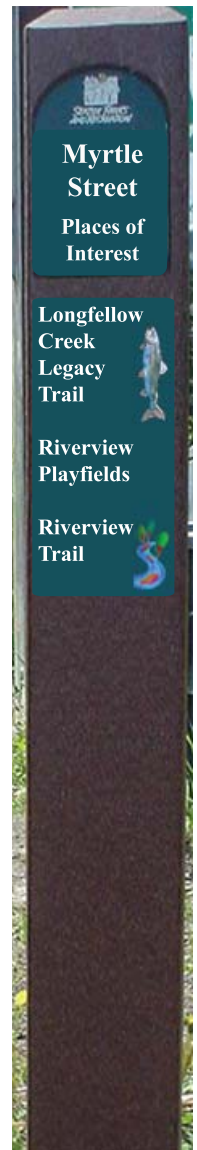
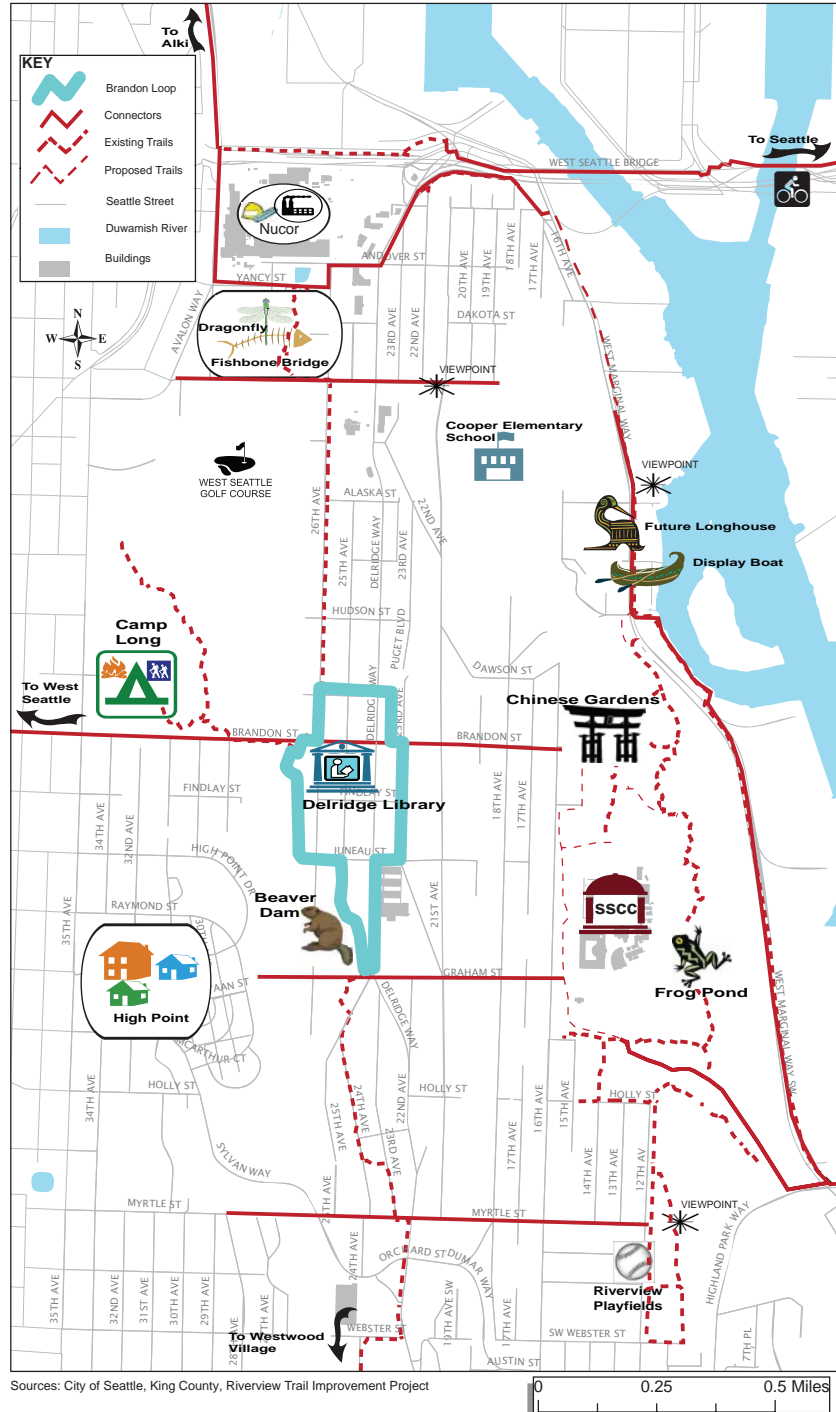




Figure 2.45: Map of Myrtle Connector

4. Implementation Strategies

4.1 Strategy 1: Community Programming and Events



A trail network is better maintained, more interesting, and safer with more people using it. Delridge will benefit from hosting a number of events that are specifically geared towards getting people to walk or bike around the neighborhood. The more people that know about the trail network, the more they will begin to use it and take ownership of it. Public ownership of these spaces will eventually lead to broader public support for improvements to the system, and perhaps maintenance. Delridge is fortunate to have the Legacy Trail as a completed example for the neighborhood to see what future trails could look or feel like. The following are a number of tools that Delridge should consider for getting people outside and walking or biking on the parts of connectors that exist now.

Sample Network Map

The following figure is an example of a trail network map for the Delridge neighborhood that DNDA could produce. This figure suggests a number of points of interest along trails and connector routes. DNDA could work with the community to decide which trails, connectors, or points of interest should appear on the map. A network map of this kind could be used as a hand-held map, in black and white. Also, elements of this map could be used on stationary maps. Community members could design the icons for the points of interest that appear on this map. Refer to Figure 2.46: Sample Network Map

Figure 2.46: Sample Network Map

Educational and Walking Tours

Walking tours are popular activities, especially if the tour incorporates a special challenge (How many stairs can you climb in an hour? Discover a new path from SSCC to the Duwamish. Do you know where Youngstown was?). Walking tours can be advertised to the greater West Seattle peninsula, on DNDA's website, and registered as National Trails Day events.

Community Clean-Up Days

A core group of neighborhood residents are very active in trail maintenance, planting, and community clean-up, particularly on the Legacy Trail and in Pigeon Point. These activities can be geared towards organized groups, or can be organized as a larger Delridge-wide event. Incorporating a day of community service with a larger event, such as a race, street scramble, or block party is a great way to simultaneously promote community spirit and improve the community.

American Hiking Society, National Trails Day (first Saturday in June)

On National Trails Day, the American Hiking Society (AHS) supports a wide variety of events that get people outside and on trails. According to their website¹³, possible events to host could include:

- Trail building or maintenance work
- Trail opening or dedication
- A hike, walk, ride, run or paddle
- A trail fair, conference, or education program
- A summertime fitness plan, including health screenings and follow-up exams

The AHS provides a detailed manual¹⁴ for organizing these kinds of events and includes such details as media contact and local event sponsorship. Events that are registered on the AHS website get free banners and t-shirts. Any of the events listed in this section would be a good way to involve the public in the Delridge trail system, and any number of these suggestions could fit into the AHS program as well.

Street Scramble

A number of neighborhoods in Seattle have a one day event called the Street Scramble.¹⁵ Participants are given a map of the area with 30 circled locations, and they must see how many of those locations they can visit in a period of time (such as 90 minutes) on foot or on bike. Participants must answer a question at each location, and those who have visited the most locations win the event. This is a great event that caters to people of all ages, incor-

13 American Hiking Society. (2004). Events and Volunteer Opportunities [WWW page]. URL <http://www.americanhiking.org/events/ntd/ideas.html>

14 American Hiking Society. (2006). National Trails Day Event Organizaer's Manual [WWW page]. <http://www.americanhiking.org/events/pdfs/2006OrgMan.pdf>.

15 Meridian Geographics. (n.d.). Street Scramble [WWW page]. URL <http://www.streetscramble.com/>

porates a competitive edge, and helps people get more familiar with their neighborhood through trivia and travel



Figure 2.47: Hoopfest in downtown Spokane, WA. Source : The Rand Puget Real Estate Team. (n.d.). [WWW page]. URL <http://www.randpuget.com/hoopic1.jpg>

Hoopfest

It is rare to drive down Delridge Way and not see an organized or impromptu basketball game going on somewhere. A number of towns have weekend basketball tournaments that take place on closed-off streets. These events can have the distinct advantage of allowing local citizens (particularly kids) to feel ownership of their neighborhoods streets. Refer to Figure 2.47: Hoopfest in Spokane, WA. An event like this in Delridge could be hosted different ways to serve different purposes:

- Limit the tournament to a few streets in the central Brandon Node, generating concentrated event activity in that area
- Spread out tournament courts on back streets and dead end rights-of-way at key locations along the trails network so that participants travel on foot through the trail network between tournament rounds. Locations could include SSCC, Cleveland High School/Louis Boren School, and High Point.

Either way, this kind of event has the potential to draw participants that might not participate in other events. This event might also include other sports or activities

Letterboxing

Letterboxing is an international treasure hunting sport that involves hiding hand-carved stamps outdoors and leaving clues (on the internet) for players to find and collect the stamps in their log books. Usually the stamp is hidden in a unique natural setting that holds special meaning to the person who hides it. Cooper Elementary, Cleveland High School, or Camp Long could consider:

- Teaching kids about participating in international letterboxing and setting up a workshop for the kids to go on a hike to find a letterbox in the Puget Sound area and then place their own letterbox in the Delridge neighborhood
- Having a local version of letterboxing adapted for youth in the neighborhood

4.2 Strategy 2: Collaboration for Artistic Wayfinding

Media

Media should be well utilized for any of these events, including major papers and local news.

- Make announcements, and send pre-written articles to newspapers, newsletters and radio stations
- Invite the media to participate
- Send pictures and pre-written articles detailing successful events

Involving Camp Long, SSCC, Cleveland High School, Youngstown Cultural Arts Center, and other Community Members

Apart from hosting or participating in any of the above events, community members can contribute to the trails network wayfinding system in Delridge. Capitalizing on the neighborhood's artist network, schools, and other community groups is a great opportunity to create original and meaningful wayfinding. The following list gives examples for how the community can take ownership of the trails system through art and design (all of these are taken from the recommendation sections).

- Give names to trails, network connectors, or staircases to go on signs and maps
- Design plaques that indicate sections of trail or staircases that have been adopted by groups or organizations
- Design or make the pedestals and the frames for signs and stationary maps
- Design or install the public art or fountain that can mark the bottoms of staircases along Delridge Way
- Adopt a trail or staircase and devote a few hours a month to clearing it of garbage, brush, and graffiti
- Design or decorate benches along trails and staircases
- Design or decorate garbage cans in the network
- Design or decorate "doggie stations" with plastic bags for cleaning up after pets
- Decide on preferred locations for picnic tables in the network (particularly at nice viewpoints, such as at the Riverview Playfields, or even at staircase landings... does the community have a nice spot they would walk to in order to eat lunch?)
- Design interpretive signs
- Locate opportunities for trail-side gardens or p-patches, and assist in the design and planting of these gardens

4.3 Strategy 3: Vegetation Management Plans

Seattle Parks and Recreation lists Lincoln Park¹⁶ and Camp Long¹⁷ as having vegetation management plans. A viewpoint plan also exists for the Riverview Playfields¹⁸. These plans delineate programs to: mitigate invasive species, plant native species, protect steep slopes from erosion, and conduct public outreach and education. The Delridge trails network would benefit by having similarly crafted plans for:

- **The West Duwamish Greenbelt/Riverview Trail system:** The Nature Consortium, in conjunction with University of Washington ecology students, is conducting ongoing restoration of this system that also includes community outreach and volunteer work. Restoration work includes a 500-acre site needing vegetation management, which can adequately be handled by one organization.
- **The Delridge neighborhood staircases:** The vegetation growing around these staircases is one of the principle reasons why they currently feel unsafe. Organized management of these staircases, and eventual adoption of staircases by community members, is one of the most immediate ways of improving their character and perceived safety. The 2006 SDOT budget for stairway maintenance is approximately \$177,000. This will fund the repair or retrofit of approximately 30 to 50 stairways, depending on the work needed for each stairway. The city has also set an additional \$375,000 aside this year for major stairway rehabilitation.

5. Prioritizing Recommendations

5.1 Criteria for Priority

Recommendations are sorted and prioritized according to a matrix of four criteria:

- **Cost:** An estimated cost was assigned to each recommendation. Green represents a recommendation that costs relatively little or that can be implemented using volunteer efforts; many community events and trail maintenance fall under this category. Yellow recommendations are moderately expensive recommendations to implement, such as stationary maps, tall trail markers, and signed bollards. Red recommendations are the most expensive recommendations, such as crosswalks and

16 Seattle Parks and Rec. (2002). Lincoln Park Vegetation Management Plan [WWW page]. URL <http://www.seattle.gov/parks/parkspaces/LincolnPark/vmp.htm>

17 Seattle Parks and Rec. (2005). Camp Long Vegetation Management Plan [WWW page]. URL <http://www.seattle.gov/parks/parkspaces/camplong/vmp.htm>

18 Seattle Parks and Rec. (2005). Riverview Palyfield Viewpoints Plan [WWW page]. URL <http://www.seattle.gov/parks/parkspaces/Viewpoints/VI-RiverviewPlayfield.pdf>

- flashing lights, trail lighting, and public art and gateway art (if it is commissioned).
- **Safety:** Green safety recommendations have a clear link to pedestrian safety, such as installing crosswalks and widening trails. Community events are also green because they populate the trails with people. Yellow safety recommendations are safety neutral. None of the recommendations are red, or detrimental to pedestrian safety.
 - **Immediate Implementation:** Green recommendations are ones that DNDA could initiate on their own, often because they involve volunteer or community efforts. Recommendations are yellow if they require coordination with outside entities, such as contracting for signed bollards or tall trail markers. Red recommendations require considerable or persistent coordination with outside agencies, for example requesting crosswalks or lighting, or extended fundraising or contracting efforts for larger projects, such as the Brandon St. hill climb.
 - **High Density, High Volume:** These recommendations are green if the connector they involve connects potential commercial, residential, and civic uses (Brandon and Graham connectors), or if they involve many community members at once, such as the events. Yellow recommendations are moderately high traffic connectors, and red recommendations are located in areas that appear to have the least traffic.

5.2 Recommendations by Time

Each matrix criterion is assigned a numbered score: green = 1, yellow = 2, and red = 3. Each recommendation then gets a total point score ranging from 4 (all four criteria are green) to 12 (all four criteria are red). We recommend the following time horizons based on the point scores:

- **0 to 2 Years:** Total score of 4, 5 or 6. These projects offer community participation at relatively low cost, are the easiest to implement or offer the safest enhancements. DNDA should consult this list for recommendations that are feasible to implement within the next two years.
- **2 to 5 Years:** Total score of 7 or 8. Most of the recommendations fall into this time horizon. These can be implemented as funding becomes available, and efforts to implement them can be initiated within two to five years. These could be considered for inclusion in the neighborhood's next comprehensive plan update.
- **5 Years and out:** Total score of 9, 10, 11 or 12 points. Some of the recommendations fall into this time horizon and ought to be considered longer range plans, particularly in the next comprehensive plan update, though efforts to coordinate and initiate these activities could begin immediately or as funding becomes available.

We encourage DNDA to carefully consider the way each recommendation has been sorted. DNDA could bring some or all of these recommendations to the community and re-prioritize them according to additional community input. In addition, any recommendation can be implemented when funding becomes available, or if a specific implementation strategy becomes apparent. These time horizons should not restrict implementation. All recommendations are additionally listed by location (connector route) and by recommended strategy, to assist easy reference to all recommendations.

Delridge Trail Network Recommendations by Implementation Horizon

connector	Location	total point categories	0-2 yrs 4 to 6	2-5 yrs 7 to 8	>5 yrs 9 to 12	cost	safety	immediate implement	high density high volume	total points	comments
0 TO 2 YEARS											
COMMUNITY EVENTS											
Street Scramble	whole network as it exists		X			green	green	green	green	4	
Hoopfest	various places in network		X			green	green	green	green	4	
Educational Walking Tours			X			green	green	green	green	4	
Community Clean-up Days			X			green	green	green	green	4	
Letterboxing			X			green	green	green	green	4	
TRAIL REPAIR AND MAINTENANCE											
Vegetation Management Plan											
	Delridge Neighborhood Staircases		X			green	green	yellow	green	5	
Remove or Move Metal Storage Shed			X			yellow	green	green	green	5	instant gratification project
Increase Trail Width											
	Brandon Connector (whole route)		X			green	green	green	green	4	
Clearing Height of 9 feet											
	Brandon Connector (whole route)		X			green	green	green	green	4	
Improve Trail Surface											
	B West of Delridge		X			yellow	green	green	green	5	
Handrails											
	Brandon Connector (35th to 16th, prioritize steep slope)		X			yellow	green	yellow	green	6	
Clear Brush											
	Genesee Staircase		X			green	green	green	yellow	5	
	B east and west of Delridge Way		X			green	green	green	green	4	
	Gr east and west of Delridge Way		X			green	green	green	green	4	
Clear Moss											
	Genesee Staircase		X			green	green	green	yellow	5	
	Gr Graham Staircases (east and west of Delridge way)		X			green	green	green	green	4	
Repair Existing Staircase chips/cracks											
	Gr Graham Staircases (east and west of Delridge way)		X			yellow	green	yellow	green	6	coordinate w/ SDOT
connector key L-D: Legacy to Duwamish, L-A: Legacy to Alki, Gr: Genesee, B: Brandon, Gr: Graham, M: Myrtle											

Delridge Trail Network Recommendations by Implementation Horizon

connector	Location	total point categories	0-2 yrs 4 to 6	2-5 yrs 7 to 8	>5 yrs 9 to 12	cost	safety	immediate implement	high density high volume	total points	comments
WAYFINDING											
Murals											
L-D	Under West Seattle Bridge		x			yellow	green	yellow	green	6	tall columns, need prof. painters, but community can design
Trash Cans											
B	Brandon and 35th		x			yellow	green	yellow	green	6	
B	Brandon and 21st		x			yellow	green	yellow	green	6	
B	where Riverview hits Marginal Way		x			yellow	green	yellow	green	6	
Signed Bollards											
L-A	N end of Legacy Trail *add directions to exiting bollard		x			green	yellow	green	yellow	6	
Repair Bench											
Gr	Graham and 2 1st (repair existing bench)		x			green	yellow	green	green	5	coordinate w/ SDOT
2 TO 5 YEARS											
WAYFINDING											
Gateways											
L-D	Delridge and W Seattle Bridge			x		red	yellow	green	green	7	Community design can start immediately
L-A	Avalon & W Seattle Bridge			x		red	yellow	green	yellow	8	
Stationary Maps											
L-A	Harbor Ave, N of W Seattle Bridge			x		yellow	yellow	yellow	yellow	8	
L-A	N end of Legacy Trail			x		yellow	yellow	yellow	green	7	
B	Brandon and Delridge			x		yellow	yellow	yellow	green	7	
Gr	Graham and High Point Dr			x		yellow	yellow	yellow	green	7	
Gr	Graham and 16th (SSCC)			x		yellow	yellow	yellow	green	7	
Signed Bollards											
L-D	Alki-Delridge intersection under bridge* = bike readable sign			x		yellow	yellow	yellow	green	7	
Gn	Genesee & 21st Ave			x		yellow	yellow	yellow	yellow	8	
Gn	Genesee & Delridge Way *include # steps			x		yellow	yellow	yellow	yellow	8	
Gn	Genesee & 26th Ave			x		yellow	yellow	yellow	green	7	
Gn	Genesee & Avalon			x		yellow	yellow	yellow	green	7	
B	Brandon & 35th			x		yellow	yellow	yellow	green	7	
B	Brandon & 26th			x		yellow	yellow	yellow	green	7	
B	Brandon & Delridge			x		yellow	yellow	yellow	green	7	
B	Brandon & 21st			x		yellow	yellow	yellow	green	7	
B	Brandon & 18th			x		yellow	yellow	yellow	green	7	
B	Brandon & 16th			x		yellow	yellow	yellow	green	7	
connector key	L-D: Legacy to Duwamish, L-A: Legacy to Alki, Gn: Genesee, B: Brandon, Gr: Graham, M: Myrtle										

Delridge Trail Network Recommendations by Implementation Horizon

connector	Location	total point categories	0-2 yrs 4 to 6	2-5 yrs 7 to 8	>5 yrs 9 to 12	cost	safety	immediate implement	high density high volume	total points	comments
Gr	Graham and 35th			x		yellow	yellow	yellow	green	7	
Gr	Graham and Delridge way			x		yellow	yellow	yellow	green	7	
Gr	Graham Staircase and Louisa Boren playground			x		yellow	yellow	yellow	green	7	
Gr	Graham and 16th			x		yellow	yellow	yellow	green	7	
M	Myrtle and Sylvan Way			x		yellow	yellow	yellow	yellow	8	
M	Myrtle and Delridge			x		yellow	yellow	yellow	yellow	8	
M	Myrtle and 21st			x		yellow	yellow	yellow	yellow	8	
M	Myrtle and 16th			x		yellow	yellow	yellow	yellow	8	
M	Myrtle and 12th			x		yellow	yellow	yellow	yellow	8	
	Tail Trail Markers										
L-D	17th & Marginal Way			x		yellow	yellow	yellow	green	7	community design
L-D	Marginal Place & W Seattle Bridge bike trail			x		yellow	yellow	yellow	green	7	
L-D	Delridge Way, south of Andover			x		yellow	yellow	yellow	green	7	
L-D	Andover & 25th			x		yellow	yellow	yellow	green	7	
L-D	Yancy & 26th			x		yellow	yellow	yellow	green	7	
L-A	Harbor Ave, north of W Seattle Bridge			x		yellow	yellow	yellow	yellow	8	
L-A	Avalon between W Seattle Bridge & Yancy			x		yellow	yellow	yellow	yellow	8	
L-A	Yancy, east of Avalon			x		yellow	yellow	yellow	yellow	8	
	Decorative bollards/stamped concrete										
	<i>Legacy Trail to Duwamish (whole route)</i>			x		yellow	yellow	yellow	green	7	
	<i>Legacy Trail to Alki (whole route)</i>			x		yellow	yellow	yellow	yellow	8	Community design can start immediately
	<i>Genesee Connector (whole route)</i>			x		yellow	yellow	yellow	yellow	8	
	<i>Brandon Connector (whole route)</i>			x		yellow	yellow	yellow	green	7	
Gr	Louisa Boren property trail (decorative bollards)			x		yellow	yellow	yellow	green	7	
Gr	Graham and 35th (stamps)			x		yellow	yellow	yellow	green	7	
Gr	Graham and Delridge (stamps)			x		yellow	yellow	yellow	green	7	
Gr	Graham and 16th (stamps)			x		yellow	yellow	yellow	green	7	
M	Myrtle and Delridge (stamps)			x		yellow	yellow	yellow	green	7	
connector key	L-D: Legacy to Duwamish, L-A: Legacy to Alki, Gr: Genesee, B: Brandon, Gr: Graham, M: Myrtle										

Delridge Trail Network Recommendations by Implementation Horizon

connector	Location	total point categories	0-2 yrs 4 to 6	2-5 yrs 7 to 8	>5 yrs 9 to 12	cost	safety	immediate implement	high density high volume	total points	comments
Public Art											
L-D	Under West Seattle Bridge			X		red	green	yellow	green	7	Community design can start immediately
Gn	Genesee & Delridge - bottom of staircase			X		red	green	yellow	yellow	8	
Gr	Graham Staircase and Louisa Boren playfield (fountain)			X		red	green	yellow	green	7	
Benches											
Gn	Genesee & 21st (landing)			X		yellow	yellow	yellow	yellow	8	
B	Brandon and 26th			X		yellow	yellow	yellow	green	7	
B	Brandon and 21st			X		yellow	yellow	yellow	green	7	
Gr	Graham and 26th			X		yellow	yellow	yellow	green	7	
Trash Cans											
Gn	Genesee & 22nd (landing)			X		yellow	green	yellow	yellow	7	
Gn	Genesee & Delridge			X		yellow	green	yellow	yellow	7	
PEDESTRIAN SAFETY											
Crosswalks											
L-D	17th and Marginal Way (pre-existing stoplight)			X		green	green	yellow	red	7	need to paint it in
Gr	Graham and Delridge (pending)			X		red	green	yellow	green	7	already in the queue
Pedestrian and Bike Signs											
L-D	17th and Marginal Way			X		yellow	green	yellow	red	8	
L-A	Harbor Ave & W Seattle Bridge on/off ramps (two locations)			X		yellow	green	yellow	yellow	7	
TRAIL REPAIR AND MAINTENANCE											
Lighting											
	Brandon Connector (35th to 16th)			X		red	green	red	green	8	coordinate w/ SPU
Improve Trail Surface											
B	16th to Marginal Way (rTrip)			X		yellow	green	green	red	7	
Add a Staircase/Hillclimb											
B	Connect 23rd to 21st			X		red	green	red	green	8	
B	Connect 17th to 16th			X		red	green	red	green	8	
Add a Trail/Path											
Gr	Through Louisa Boren playfield			X		yellow	yellow	yellow	green	7	coordinate w/ school district
Vegetation Management Plan											
	Duwamish Greenbelt			X		green	green	yellow	red	7	coordinate w/ parks and rec
connector key L-D: Legacy to Duwamish, L-A: Legacy to Alki, Gn: Genesee, B: Brandon, Gr: Graham, M: Myrtle											

Delridge Trail Network Recommendations by Implementation Horizon											
connector	Location	total point categories	0-2 yrs 4 to 6	2-5 yrs 7 to 8	>5 yrs 9 to 12	cost	safety	immediate implement	high density high volume	total points	comments
> 5 YEARS											
WAYFINDING											
Stationary Maps											
L-D	17th & Marginal Way				x	yellow	yellow	yellow	red	9	
B	where Riverview hits Marginal Way				x	yellow	yellow	yellow	red	9	
PEDESTRIAN SAFETY											
Crosswalks											
L-A	Harbor Ave & W Seattle Bridge on/off ramps (two crosswalks)				x	red	green	red	yellow	9	
Flashing lights											
L-D	17th and Marginal Way				x	red	green	red	red	10	
connector key L-D: Legacy to Duwamish, L-A: Legacy to Alki, Gn: Genesee, B: Brandon, Gr: Graham, M: Myrtle											

Delridge Trail Network Recommendations By Connector										
Location	total point categories	0-2 yrs 4 to 6	2-5 yrs 7 to 8	>5 yrs 9 to 12	cost	safety	immediate implement	high density high volume	total points	comments
Legacy Trail to Duwamish Gateway								green = 1 yellow = 2 red = 3		
Delridge and W Seattle Bridge			x		red	yellow	green	green	7	community design
Stationary Map										
17th & Marginal Way				x	yellow	yellow	yellow	red	9	
Bike-readable sign										
Alki-Delridge intersection under bridge			x		yellow	yellow	yellow	green	7	
Tall Trail Markers										
17th & Marginal Way			x		yellow	yellow	yellow	green	7	community design
Marginal Place & W Seattle Bridge bike trail			x		yellow	yellow	yellow	green	7	
Delridge Way, south of Andover			x		yellow	yellow	yellow	green	7	
Andover & 25th			x		yellow	yellow	yellow	green	7	
Yancy & 26th			x		yellow	yellow	yellow	green	7	
Decorative bollards/stamped concrete (whole route)										
Mural										
Under West Seattle Bridge		x			yellow	green	yellow	green	6	community design
Public Art Installation										
Under West Seattle Bridge			x		red	green	yellow	green	7	community design
Crosswalk (paint where pre-existing stop light)										
17th and Marginal Way (pre-existing stoplight)			x		green	green	yellow	red	7	need to paint it in
"Pedestrian and Bike" signs (for cars)										
17th and Marginal Way			x		yellow	green	yellow	red	8	
Flashing Crosswalk Lights										
17th and Marginal Way				x	red	green	red	red	10	
Legacy Trail to Alki Gateway										
Avalon & W Seattle Bridge			x		red	yellow	green	yellow	8	community design
Stationary Maps										
Harbor Ave, N of W Seattle Bridge			x		yellow	yellow	yellow	yellow	8	
N end of Legacy Trail			x		yellow	yellow	yellow	green	7	

Delridge Trail Network Recommendations By Connector

Location	total point categories	0-2 yrs 4 to 6	2-5 yrs 7 to 8	>5 yrs 9 to 12	cost	safety	immediate implement	high density high volume	total points	comments
Signed Bollard										
L-A N end of Legacy Trail *add directions to exiting bollard		x			green	yellow	green	yellow	6	
Tall Trail Markers										
L-A Harbor Ave, north of W Seattle Bridge			x		yellow	yellow	yellow	yellow	8	
L-A Avalon between W Seattle Bridge & Yancy			x		yellow	yellow	yellow	yellow	8	
L-A Yancy, east of Avalon			x		yellow	yellow	yellow	yellow	8	
Decorative bollards/stamped concrete (whole route)			x		yellow	yellow	yellow	yellow	8	community design
Crosswalk										
Harbor Ave & W Seattle Bridge on/off ramps (two crosswalks)				x	red	green	red	yellow	9	
"Pedestrian and Bike" signs (for cars)										
Harbor Ave & W Seattle Bridge on/off ramps (two locations)			x		yellow	green	yellow	yellow	7	
Genesee Connector										
Signed Bollards										
Genesee & 21st Ave			x		yellow	yellow	yellow	yellow	8	
Genesee & Delridge Way *include # steps			x		yellow	yellow	yellow	yellow	8	
Genesee & 26th Ave			x		yellow	yellow	yellow	green	7	
Genesee & Avalon			x		yellow	yellow	yellow	green	7	
Decorative bollards/stamped concrete (whole route)			x		yellow	yellow	yellow	yellow	8	
Public Art										
Genesee & Delridge - bottom of staircase			x		red	green	yellow	yellow	8	community design
Bench										
Genesee & 21st (landing)			x		yellow	yellow	yellow	yellow	8	
Trash Cans										
Genesee & 22nd (landing)			x		yellow	green	yellow	yellow	7	community design
Genesee & Delridge			x		yellow	green	yellow	yellow	7	
Clear Brush										
Genesee Staircase		x			green	green	green	yellow	5	
Clear Moss										
Genesee Staircase		x			green	green	green	yellow	5	

Delridge Trail Network Recommendations By Connector										
Location	total point categories	0-2 yrs 4 to 6	2-5 yrs 7 to 8	>5 yrs 9 to 12	cost	safety	immediate implement	high density high volume	total points	comments
Brandon Connector										
<i>Stationary Map</i>										
Brandon and Delridge where Riverview hits Marginal Way			x	x	yellow yellow	yellow yellow	yellow yellow	green red	7 9	
<i>Signed Bollards</i>										
Brandon & 35th			x		yellow	yellow	yellow	green	7	
Brandon & 26th			x		yellow	yellow	yellow	green	7	
Brandon & Delridge			x		yellow	yellow	yellow	green	7	
Brandon & 21st			x		yellow	yellow	yellow	green	7	
Brandon & 18th			x		yellow	yellow	yellow	green	7	
Brandon & 16th			x		yellow	yellow	yellow	green	7	
<i>Decorative bollards/stamped concrete (whole route)</i>										
<i>Benches</i>										
Brandon & 26th			x		yellow	yellow	yellow	green	7	
Brandon and 21st			x		yellow	yellow	yellow	green	7	
<i>Trash Cans</i>										
Brandon & 35th		x			yellow	green	yellow	green	6	community design
Brandon and 21st		x			yellow	green	yellow	green	6	
where Riverview hits Marginal Way		x			yellow	green	yellow	green	6	
<i>Clear Brush</i>										
east and west of Delridge Way		x			green	green	green	green	4	
<i>Install Handrails (35th to 16th, prioritize steep slope)</i>										
Install Lighting (35th to 16th)		x			yellow	green	yellow	green	6	
Increase Trail Width (whole route)		x	x		red	green	red	green	8	coordinate w/ SPU
Clearing Height of 9 feet (whole route)		x			green	green	green	green	4	
<i>Improve Trail Surface</i>										
West of Delridge		x			yellow	green	green	green	5	
16th to Marginal Way (rTrip - Riverview Trail)			x		yellow	green	green	red	7	
<i>Add a staircase / hillclimb</i>										
Connect 23rd to 21st			x		red	green	red	green	8	
Connect 17th to 16th			x		red	green	red	green	8	
Graham Connector										
<i>Stationary Map</i>										
Graham and High Point Dr			x		yellow	yellow	yellow	green	7	
Graham and 16th			x		yellow	yellow	yellow	green	7	

Delridge Trail Network Recommendations By Connector

Location	total point categories	0-2 yrs 4 to 6	2-5 yrs 7 to 8	>5 yrs 9 to 12	cost	safety	immediate implement	high density high volume	total points	comments
Signed Bollards										
Graham and 35th			x		yellow	yellow	yellow	green	7	
Graham and Delridge way			x		yellow	yellow	yellow	green	7	
Graham Staircase and Louisa Boren Playfield			x		yellow	yellow	yellow	green	7	
Graham and 16th			x		yellow	yellow	yellow	green	7	
Decorative Bollards										
Louisa Boren playfield trail			x		yellow	yellow	yellow	green	7	community design
Stamped Concrete										
Graham and 35th			x		yellow	yellow	yellow	green	7	community design
Graham and Delridge			x		yellow	yellow	yellow	green	7	
Graham and 16th			x		yellow	yellow	yellow	green	7	
Fountain										
Graham Staircase and Louisa Boren Playfield			x		red	green	yellow	green	7	community design
Add a Bench										
Graham and 26th			x		yellow	yellow	yellow	green	7	
Repair existing Bench										
Graham and 21st (repair existing bench)		x			green	yellow	green	green	5	coordinate w/ SDOT
Crosswalk (pending)										
Graham and Delridge (pending)			x		red	green	yellow	green	7	already in the queue
Clear Brush										
Graham Staircases east and west of Delridge Way		x			green	green	green	green	4	
Clear Moss										
Graham Staircases (east and west of Delridge way)		x			green	green	green	green	4	
Repair existing staircase chips and cracks										
Graham Staircases (east and west of Delridge way)		x			yellow	green	yellow	green	6	coordinate w/ SDOT
Move metal storage shed										
Graham Staircase and Louisa Boren Playfield		x			yellow	green	green	green	5	instant gratification project
Add a Trail										
Through Louisa Boren playfield			x		yellow	yellow	yellow	green	7	coordinate w/ school district
Myrtle Connector										
Signed Bollards										
Myrtle and Sylvan Way			x		yellow	yellow	yellow	yellow	8	
Myrtle and Delridge			x		yellow	yellow	yellow	yellow	8	
Myrtle and 21st			x		yellow	yellow	yellow	yellow	8	
Myrtle and 16th			x		yellow	yellow	yellow	yellow	8	
Myrtle and 12th			x		yellow	yellow	yellow	yellow	8	
Stamped Concrete										
Myrtle and Delridge			x		yellow	yellow	yellow	yellow	8	

Delridge Trail Network Recommendations by Strategy

connector	Location	0-2 yrs total point categories 4 to 6	2-5 yrs 7 to 8	>5 yrs 9 to 12	cost	safety	immediate implement	high density high volume	total points	comments
Wayfinding										
Gateways										
L-D	Delridge and W Seattle Bridge		x				green	green	7	
L-A	Avalon & W Seattle Bridge		x		red	yellow	green	yellow	8	community design
Stationary Maps										
L-D	17th & Marginal Way			x	yellow	yellow	yellow	red	9	
L-A	Harbor Ave, N of W Seattle Bridge		x		yellow	yellow	yellow	yellow	8	
L-A	N end of Legacy Trail		x		yellow	yellow	yellow	green	7	
B	Brandon and Delridge		x		yellow	yellow	yellow	green	7	
B	where Riverview hits Marginal Way			x	yellow	yellow	yellow	red	9	
Gr	Graham and High Point Dr		x		yellow	yellow	yellow	green	7	
Gr	Graham and 16th		x		yellow	yellow	yellow	green	7	
Signed Bollards										
L-D	Alki-Delridge intersection under bridge* = bike readable sign		x		yellow	yellow	yellow	green	7	
L-A	N end of Legacy Trail *add directions to exiting bollard	x			green	yellow	green	yellow	6	
Gn	Genesee & 21st Ave		x		yellow	yellow	yellow	yellow	8	
Gn	Genesee & Delridge Way *include # steps		x		yellow	yellow	yellow	yellow	8	
Gn	Genesee & 26th Ave		x		yellow	yellow	yellow	green	7	
Gn	Genesee & Avalon		x		yellow	yellow	yellow	green	7	
B	Brandon & 35th		x		yellow	yellow	yellow	green	7	
B	Brandon & 26th		x		yellow	yellow	yellow	green	7	
B	Brandon & Delridge		x		yellow	yellow	yellow	green	7	
B	Brandon & 21st		x		yellow	yellow	yellow	green	7	
B	Brandon & 18th		x		yellow	yellow	yellow	green	7	
B	Brandon & 16th		x		yellow	yellow	yellow	green	7	
Gr	Graham and 35th		x		yellow	yellow	yellow	green	7	
Gr	Graham and Delridge way		x		yellow	yellow	yellow	green	7	
Gr	Graham Staircase and Louisa Boren playfield		x		yellow	yellow	yellow	green	7	
Gr	Graham and 16th		x		yellow	yellow	yellow	green	7	
M	Myrtle and Sylvan Way		x		yellow	yellow	yellow	yellow	8	
M	Myrtle and Delridge		x		yellow	yellow	yellow	yellow	8	
M	Myrtle and 21st		x		yellow	yellow	yellow	yellow	8	
M	Myrtle and 16th		x		yellow	yellow	yellow	yellow	8	
M	Myrtle and 12th		x		yellow	yellow	yellow	yellow	8	
connector key L-D: Legacy to Duwamish, L-A: Legacy to Alki, Gn: Genesee, B: Brandon, Gr: Graham, M: Myrtle										

Delridge Trail Network Recommendations by Strategy

connector	Location	total point categories	0-2 yrs 4 to 6	2-5 yrs 7 to 8	>5 yrs 9 to 12	cost	safety	immediate implement	high density high volume	total points	comments
Tail Trail Markers											
L-D	17th & Marginal Way			x		yellow	yellow	yellow	green	7	community design
L-D	Marginal Place & W Seattle Bridge bike trail			x		yellow	yellow	yellow	green	7	
L-D	Delridge Way, south of Andover			x		yellow	yellow	yellow	green	7	
L-D	Andover & 25th			x		yellow	yellow	yellow	green	7	
L-D	Yancy & 26th			x		yellow	yellow	yellow	green	7	
L-A	Harbor Ave, north of W Seattle Bridge			x		yellow	yellow	yellow	yellow	8	
L-A	Avalon between W Seattle Bridge & Yancy			x		yellow	yellow	yellow	yellow	8	
L-A	Yancy, east of Avalon			x		yellow	yellow	yellow	yellow	8	
Decorative bollards/stamped concrete											
	Legacy Trail to Duwamish (whole route)			x		yellow	yellow	yellow	green	7	community design
	Legacy Trail to Alki (whole route)			x		yellow	yellow	yellow	yellow	8	
	Genesee Connector (whole route)			x		yellow	yellow	yellow	yellow	8	
	Brandon Connector (whole route)			x		yellow	yellow	yellow	green	7	
Gr	Louisa Boren property trail (decorative bollards)			x		yellow	yellow	yellow	green	7	
Gr	Graham and 35th (stamps)			x		yellow	yellow	yellow	green	7	
Gr	Graham and Delridge (stamps)			x		yellow	yellow	yellow	green	7	
Gr	Graham and 16th (stamps)			x		yellow	yellow	yellow	green	7	
M	Myrtle and Delridge (stamps)			x		yellow	yellow	yellow	yellow	8	
Murals											
L-D	Under West Seattle Bridge		x			yellow	green	yellow	green	6	community design
Public Art											
L-D	Under West Seattle Bridge			x		red	green	yellow	green	7	community design
Gn	Genesee & Delridge - bottom of staircase			x		red	green	yellow	yellow	8	
Gr	Graham Staircase and Louisa Boren playfield (fountain)			x		red	green	yellow	green	7	
Benches											
Gn	Genesee & 21st (landing)			x		yellow	yellow	yellow	yellow	8	
B	Brandon & 26th			x		yellow	yellow	yellow	green	7	
B	Brandon and 21st			x		yellow	yellow	yellow	green	7	
Gr	Graham and 26th			x		yellow	yellow	yellow	green	7	
Gr	Graham and 21st (repair existing bench)		x			green	yellow	green	green	5	coordinate w/ SDOT
connector key L-D: Legacy to Duwamish, L-A: Legacy to Alki, Gn: Genesee, B: Brandon, Gr: Graham, M: Myrtle											

Delridge Trail Network Recommendations by Strategy											
connector	Location	total point categories	0-2 yrs 4 to 6	2-5 yrs 7 to 8	>5 yrs 9 to 12	cost	safety	immediate implement	high density high volume	total points	comments
Trash Cans											
Gn	Genesee & 22nd (landing)			x		yellow	green	yellow	yellow	7	community design
Gn	Genesee & Delridge			x		yellow	green	yellow	yellow	7	
B	Brandon & 35th		x			yellow	green	yellow	green	6	
B	Brandon and 21st		x			yellow	green	yellow	green	6	
B	where Riverview hits Marginal Way		x			yellow	green	yellow	green	6	
PEDESTRIAN SAFETY											
Crosswalks											
L-D	17th and Marginal Way (pre-existing stoplight)			x		green	green	yellow	red	7	need to paint it in
L-A	Harbor Ave & W Seattle Bridge on/off ramps (two crosswalks)				x	red	green	red	yellow	9	
Gr	Graham and Delridge (pending)			x		red	green	yellow	green	7	already in the queue
Pedestrian and Bike Signs											
L-D	17th and Marginal Way			x		yellow	green	yellow	red	8	
L-A	Harbor Ave & W Seattle Bridge on/off ramps (two locations)			x		yellow	green	yellow	yellow	7	
Flashing lights											
L-D	17th and Marginal Way				x	red	green	red	red	10	
TRAIL REPAIR AND MAINTENANCE											
Clear Brush											
Gn	Genesee Staircase		x			green	green	green	yellow	5	
B	east and west of Delridge Way		x			green	green	green	green	4	
Gr	east and west of Delridge Way		x			green	green	green	green	4	
Clear Moss											
Gn	Genesee Staircase		x			green	green	green	yellow	5	
Gr	Graham Staircases (east and west of delridge way)		x			green	green	green	green	4	
Repair Existing Staircase chips/cracks											
Gr	Graham Staircases (east and west of delridge way)		x			yellow	green	yellow	green	6	coordinate w/ SDOT
Handrails											
Brandon Connector	(35th to 16th) * Prioritize steep slopes		x			yellow	green	yellow	green	6	
Lighting											
Brandon Connector	(35th to 16th)			x		red	green	red	green	8	coordinate w/ SPU
Increase Trail Width to 6 feet											
Brandon Connector	(whole route)		x			green	green	green	green	4	
connector key L-D: Legacy to Duwamish, L-A: Legacy to Alki, Gn: Genesee, B: Brandon, Gr: Graham, M: Myrtle											

Delridge Trail Network Recommendations by Strategy											
connector	Location	total point categories	0-2 yrs 4 to 6	2-5 yrs 7 to 8	>5 yrs 9 to 12	cost	safety	immediate implement	high density high volume	total points	comments
Clearing Height of 9 feet											
<i>Brandon Connector</i>	(whole route)		x			green	green	green	green	4	
Improve Trail Surface											
<i>B</i>	West of Delridge		x			yellow	green	green	green	5	
<i>B</i>	16th to Marginal Way (rTrip)			x		yellow	green	green	red	7	
Add a Staircase/Hilclimb											
<i>B</i>	Connect 23rd to 21st			x		red	green	red	green	8	
<i>B</i>	Connect 17th to 16th			x		red	green	red	green	8	
Remove Metal Storage Shed											
<i>Gr</i>	Graham Staircase and Louisa Boren playground		x			yellow	green	green	green	5	instant gratification project
Add a Trail/Path											
<i>Gr</i>	Through Louisa Boren playground			x		yellow	yellow	yellow	green	7	coordinate w/ school district
Vegetation Management Plan											
	Duwamish Greenbelt			x		green	green	yellow	red	7	coordinate w/ parks and rec
	Delridge Neighborhood Staircases		x			green	green	yellow	green	5	
COMMUNITY EVENTS											
Street Scramble											
	whole network as it exists		x			green	green	green	green	4	
Hoopfest											
	various places in network		x			green	green	green	green	4	
Educational Walking Tours											
			x			green	green	green	green	4	
Community Clean-up Days											
			x			green	green	green	green	4	
Letterboxing											
			x			green	green	green	green	4	
<i>connector key L-D: Legacy to Duwamish, L-A: Legacy to Alki, Gn: Genesee, B: Brandon, Gr: Graham, M: Myrtle</i>											